

The Village

SPECIFIC PLAN



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Second Amendment

City of Oxnard, CA

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TABLE OF CONTENTS

	<u>PAGE</u>	
1.0	<u>INTRODUCTION</u>	
1.1	PROEJCT SUMMARY	1-1
1.2	PURPOSE OF THE SPECIFIC PLAN	1-5
1.3	LEGAL ASPECTS	1-5
1.4	COMMUNITY SETTING	1-11
1.5	PROPOSED LAND USE AND UTILITIES	1-15
1.6	ORGANIZATION OF SPECIFIC PLAN	1-15
2.0	<u>LAND USE PLAN</u>	
2.1	INTRODUCTION	2-1
2.2	LAND USE CONCEPT	2-1
2.3	LAND USE CONSISTENCY	2-14
2.4	LAND USE REGULATION	2-14
2.5	LAND USE FLEXIBILITY	2-15
3.0	<u>MIXED USE DEVELOPMENT PLAN</u>	
3.1	INTRODUCTION	3-1
3.2	DEVELOPMENT CHARACTER	3-1
3.3	MANDATORY MIXED USE DEVELOPMENT STANDARDS	3-6
3.4	BUILDING TYPES	3-10
3.5	FRONTAGE TYPES	3-17
4.0	<u>RESIDENTIAL DEVELOPMENT PLAN</u>	
4.1	INTRODUCTION	4-1
4.2	DEVELOPMENT CHARACTER	4-1
4.3	MANDATORY RESIDENTIAL DESIGN ELEMENTS	4-3
4.4	BUILDING TYPES	4-8
4.5	FRONTAGE TYPES	4-19
5.0	<u>LANDSCAPE DEVELOPMENT PLAN</u>	
5.1	INTRODUCTION	5-1
5.2	MANDATORY LANDSCAPE DESIGN PRINCIPLES	5-1
5.3	STREETSCAPES	5-4
5.4	CONCEPTUAL PARK DESIGNS	5-12
5.5	CONCPTUAL EDGE AND ENTRY LANDSCAPE DESIGNS	5-19
5.6	NATURAL STORM WATER MANAGEMENT	5-28
5.7	LANDSCAPING PLANT MATERIALS	5-30

	<u>PAGE</u>	
6.0	<u>INFRASTRUCTURE DEVELOPMENT PLAN</u>	
6.1	INTRODUCTION	6-1
6.2	DEMOLITION AND GRADING	6-1
6.3	DRAINAGE AND WATER QUALITY	6-3
6.4	WATER SERVICE	6-8
6.5	WASTEWATER SERVICE	6-10
6.6	RECYLCED WATER	6-11
6.7	DRY UTILITIES	6-13
6.8	ROADWAY DESIGN	6-13
6.9	PARKING	6-15
6.10	TRANSPORTATION DEMAND MANAGEMENT (TDM)	6-21
7.0	<u>DESIGN AND ARCHITECTURAL STANDARDS AND GUIDELINES</u>	
7.1	INTRODUCTION	7-1
7.2	RESIDENTIAL DESIGN STANDARDS AND GUIDELINES	7-8
7.3	ARCHITECTURAL GUIDELINES	7-24
7.4.	SUSTAINABILITY DESIGN GUIDELINES AND STANDARDS	7-30
7.5	LANDSCAPE DESIGN STANDARDS AND GUIDELINES	7-31
7.6	SIGNAGE STANDARDS	7-43
7.7	EXTERIOR LIGHTING STANDARDS	7-52
8.0	<u>ADMINISTRATION AND IMPLEMENTATION</u>	
8.1	INTRODUCTION AND APPLICATION	8-1
8.2	REGULATORY OBJECTIVES	8-1
8.3	ADOPTION, ADMINISTRATION, AND RELATED PUBLIC ACTIONS	8-2
8.4	IMPLEMENTATION RESPONSIBILITIES	8-3
8.5	DEVELOPMENT PHASING AND SCHEDULING	8-5
8.6	FINANCING AND FUNDING	8-7
8.7	PROJECT AND SUBDIVISION MAP APPROVAL PROCESS	8-10
8.8	OTHER ISSUES	8-15
9.0	<u>GENERAL PLAN CONSISTENCY</u>	
9.1	INTRODUCTION	9-1
9.2	GROWTH MANAGEMENT ELEMENT	9-1
9.3	LAND USE ELEMENT	9-2
9.4	CIRCULATION ELEMENT	9-4

	<u>PAGE</u>
9.5 PUBLIC FACILITIES ELEMENT	9-5
9.6 OPEN SPACE AND CONSERVATION ELEMENT	9-6
9.7 SAFETY ELEMENT	9-7
9.8 NOISE ELEMENT	9-7
9.9 ECONOMIC DEVELOPMENT ELEMENT	9-8
9.10 COMMUNITY DESIGN ELEMENT	9-9
9.11 PARKS AND RECREATION ELEMENT	9-10
9.12 HOUSING ELEMENT	9-11

TABLES

2.1 PARKING SUMMARY	2-12
2.2 LAND USE SUMMARY	2-18
2.3 LAND USE SUMMARY FOR DEVELOPMENT OPTION "A"	2-19
2.4 SUMMARY OF PERMITTED USES BY LAND USE CATEGORY	2-20
5.1 LANDSCAPE PLANT MATERIALS LIST	5-31
7.1 EXAMPLES OF APPROPRIATE STREET FURNISHINGS AND HARDWARE	7-40
7.2 COMMERCIAL TOP OF BUILDING WALL IDENTITY SIGN PARAMETERS	7-47

FIGURES

1.0	<u>INTRODUCTION</u>	
1.1	REGIONAL LOCATION	1-3
1.2	SPECIFIC PLAN VICINITY	1-4
1.3	SPECIFIC PLAN BOUNDARY	1-9
1.4	2020 GENERAL PLAN LAND USE MAP	1-10
1.5	EXISTING USES	1-14
2.0	<u>LAND USE PLAN</u>	
2.1	LAND USE PLAN	2-5
2.2	ILLUSTRATIVE SITE PLAN	2-6
2.3	VEHICLE CIRCULATION CONCEPT	2-8
2.4	PEDESTRIAN CIRCULATION CONCEPT	2-10
2.5	BICYCLE CIRCULATION CONCEPT	2-11
2.6	SUMMARY OF DEVELOPMENT OPTIONS	2-17

	<u>PAGE</u>	
3.0	<u>MIXED USE DEVELOPMENT PLAN</u>	
3.1	PLANNING AREA 17 DESIGN CHARACTER	3-3
3.2	PLANNING AREA 18 DESIGN CHARACTER	3-4
3.3	PLANNING AREA 19 (TRANSIT CENTER) DESIGN CHARACTER	3-5
3.4	MADATORY MIXED USE DESIGN ELEMENTS	3-9
3.5	MIXED USE REGULATING PLAN	3-21
4.0	<u>RESIDENTIAL DEVELOPMENT PLAN</u>	
4.1	MANDATORY RESIDENTIAL DESIGN ELEMENTS	4-7
4.2	RESIDENTIAL REGULATING PLAN	4-22
5.0	<u>LANDSCAPE DEVELOPMENT PLAN</u>	
5.1	LANDSCAPING AREAS AND MAINTENANCE RESPONSIBILITIES	5-3
5.2	ROADWAY NETWORK	5-6
5.3	MAIN STREET COMMERCIAL CROSS-SECTION	5-7
5.4	MAIN STREET RESIDENTIAL CROSS-SECTION	5-8
5.5	NEIGHBORHOOD STREET CROSS-SECTION	5-9
5.6	ALLEY STREET CROSS SECTION	5-10
5.7	PROPOSED TRAFFIC CALMING MECHANISMS	5-11
5.8	VILLAGE GREEN	5-13
5.9	NEIGHBORHOOD PARK	5-14
5.10	POCKET PARKS	5-15
5.11	PASEOS, PLAZAS, AND PATHWAYS	5-16
5.12	COURTYARD AND TERRACE RECREATIONAL FACILITIES	5-17
5.13	US HIGHWAY 101 FREEWAY LANDSCAPING	5-21
5.14	WAGON WHEEL ROAD OFF-RAMP GATEWAY LANDSCAPING	5-22
5.15	UNION PACIFIC RAILROAD EDGE CROSS-SECTION	5-23
5.16	OXNARD BOULEVARD CROSS-SECTION	5-24
5.17	OXNARD BOULEVARD ENTRY LANDSCAPING	5-25
5.18	VENTURA ROAD CROSS-SECTION	5-26
5.19	VENTURA ROAD ENTRY LANDSCAPING	5-27
5.20	NATURAL LANDSCAPING STORM WATER MANAGEMENT OPPORTUNITIES	5-29
5.21	STREET TREE PLAN	5-36
6.0	<u>INFRASTRUCTURE DEVELOPMENT PLAN</u>	
6.1	DEMOLITION PLAN	6-2
6.2	POTENTIAL STORM WATER MANAGEMENT DESIGN SOLUTIONS	6-7
6.3	PRELIMINARY BACKBONE INFRASTRUCTURE PLAN	6-12
6.4	PARKING PLAN	6-20
6.5	SUMMARY OF TDM PROGRAM COMPONENTS	6-24

	<u>PAGE</u>	
7.0	<u>DESIGN AND ARCHITECTURAL STANDARDS AND GUIDELINES</u>	
7.1	WAGON WHEEL SUB-TRANSPORTATION CENTER DESIGN CONEPT	7-23
8.0	<u>ADMINISTRATION AND IMPLEMENTATION</u>	
8.1	CONCEPTUAL PHASING PLAN	8-7
8.2	REVIEW PROCESS FLOW CHART	8-14



Chapter 1

The Village

SPECIFIC PLAN



Introduction

CHAPTER 1.0

Introduction

1.1 PROJECT SUMMARY

The Village Specific Plan is a 63-acre pedestrian and transit-oriented development located in the Wagon Wheel area of Oxnard, CA (refer to Figures 1.1 and 1.2). Located within the City's Historic Enhancement and Revitalization of Oxnard (HERO) redevelopment area, the Village Specific Plan will transform a patchwork of unplanned uses into a cohesive mix of commercial and residential uses. The intent of the overall design theme is to create a village environment with European architectural styles and architectural styles found throughout Oxnard and greater Ventura County. Planned uses include a well organized mix of neighborhood serving retail, restaurants, commercial office, live-work, residential, transit, and recreational opportunities.

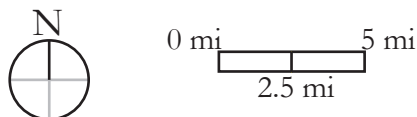
The Village Specific Plan objectives were derived primarily from Smart Growth guiding principles, the context of the surrounding residential and commercial environments, as well as the City of Oxnard's desire to promote redevelopment within the HERO district. The Village plan will extend benefits far beyond its boundaries by transforming this economically depressed site into a vibrant urban village that will offer new opportunities for shopping, dining, employment, housing, and alternative modes of transportation in close proximity to one another. Specifically, The Village Specific Plan is designed to achieve the following objectives:

- Promote redevelopment of the former Wagon Wheel site consistent with the goals of the HERO district;
- Shift the land use emphasis from single-use residential, industrial and commercial into a vibrant mix of residential, neighborhood serving commercial, recreational, and transit-oriented development that supports rather than undermines the regional commercial market;
- Provide a variety of housing types to accommodate and attract a wide range of household types (singles, newlyweds, families with children, and seniors) and income levels;
- Create great streets designed for the automobile, pedestrians and bicyclists;
- Replace the existing discontinuous roadway system with a curvilinear "Main Street" which interconnects the residential neighborhoods with the neighborhood commercial services;
- Create ample opportunities for recreation, such as community greens and recreation centers, memorable public gathering spaces, and pedestrian and bicycle pathways and linkages;

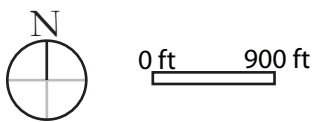
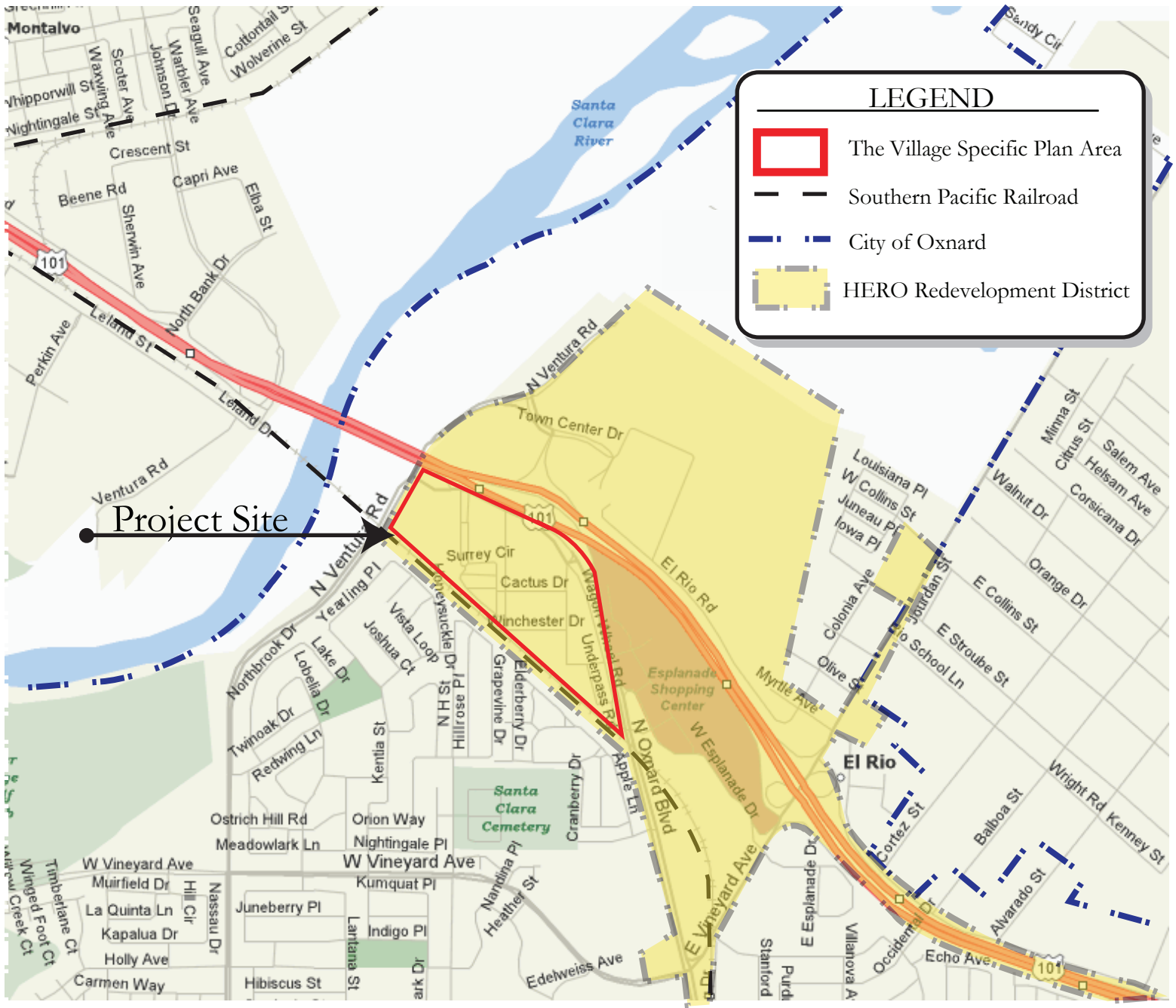
CHAPTER 1.0

Introduction

- Create rich and attractive landscape environments that are inviting and that promote social interaction and pedestrian oriented recreational activity;
- Promote more sustainable living through the construction of a multi-modal sub-transportation center, natural storm water pollution prevention, water conserving landscaping and use of resource-efficient materials;
- Creatively improve existing storm water filtration and drainage conditions by constructing on-site storm water treatment facilities and drainage system improvements;
- Calm traffic through the use of pedestrian friendly streetscapes and traffic calming mechanisms; and
- Establish prominent gateway landscaping and monument signage for the City of Oxnard.



Regional Location



Specific Plan Vicinity

CHAPTER 1.0

Introduction

1.2 PURPOSE OF THE SPECIFIC PLAN

The purpose of The Village Specific Plan is to provide the City of Oxnard with a comprehensive planning program to regulate the use of land and govern the orderly development of The Village.

The Village Specific Plan proposes a conceptual land use plan, as well as other regulations in the form of “form-based” architectural and building standards and guidelines. These are intended to insure two results: a) that development within the Specific Plan area will be consistent with the goals, objectives, principles and policies of the City of Oxnard 2020 General Plan, and b) that future development of the private and public realm of the Specific Plan area will be in harmony with the vision, design intent and objectives of this plan.

This Specific Plan establishes the development concept and development regulations, which will govern future redevelopment of the project area over time. When development projects are brought before the City of Oxnard, the planning staff will use this Specific Plan as the primary guide for project review. Each development phase must be consistent, and in substantial conformance, with the vision, spirit, intent, objectives and regulations of this Specific Plan. In situations where policies or standards relating to a particular subject have not been provided in the Specific Plan, the existing policies and standards of the

City of Oxnard General Plan and the City of Oxnard Zoning Ordinance will continue to apply.

1.3 LEGAL ASPECTS

1.3.1 Contents

This Specific Plan contains text and diagrams, which illustrate in detail:

- The location and extent of land uses proposed within The Village Specific Plan Area;
- The location, extent, and general intensity of major infrastructure components, including but not limited to, public and private transportation, wastewater, water drainage, solid waste disposal, energy, recycled water, and other essential facilities planned to support the land uses described in this Specific Plan;
- Criteria by which development will proceed, including “form-based” development standards, architectural design guidelines, and a phasing program; and
- A program of implementation measures, including design and development review procedures, required public works projects, and financing measures.

The Village Specific Plan also establishes the conceptual layout and design of the streets and the location of Planning Areas, to which land use, urban design,

CHAPTER 1.0

Introduction

architectural, and landscaping standards apply.

At such time when a designated phase is proposed for development under the provisions of this Specific Plan, detailed subdivision maps, grading plans, architectural plans, and building plans shall precisely illustrate the roadway alignments and other associated public improvements (such as landscaping, drainage, utilities, etc.), and the precise configuration of commercial and residential building forms, recreational amenities, and transit facilities within each development parcel.

1.3.2 Consistency

The Village Specific Plan is consistent with the California Government Code and subject to conditions noted below. It is also consistent with the City of Oxnard 2020 General Plan.

California Government Code: This Specific Plan has been prepared in accordance with the requirements of the California Government Code (Sections 65450 through 65457). This Code establishes a legal mechanism, termed a “Specific Plan,” which allows an area to be established whose development rules supersede any previously established zoning. Specific plans are developed to provide and ensure a greater level of detail in planning projects of special interest or value to a community within a defined area called a specific plan area. The plan typically contains a set of medium and long-range policies for

land use and development in a defined project.

Specific plans are required to contain text and diagrams specifying:

- Location and extent of land uses,
- Location and extent of major infrastructure improvements needed to support the land uses described in the plans,
- Development standards and criteria,
- A program of implementation measures,
- A statement of relationship between the Specific Plan and the General Plan, and
- Any other subjects that are necessary or desirable for implementation of the General Plan.

After adoption, a specific plan has an effect within the specific plan area similar to that of the general plan for the city as a whole. For example, the State’s Subdivision Map Act requires the legislative body to deny approval of a tentative or final subdivision map if it is not in substantial conformance with any applicable specific plan. In addition, a development agreement between a municipal body and a developer/applicant cannot be approved unless the legislative body finds the agreement is consistent with the general plan and any applicable specific plan.

City of Oxnard 2020 General Plan: The Village Specific Plan provides a land use plan and supporting regulations and guidelines to ensure development in a manner consistent with the goals,

CHAPTER 1.0

Introduction

objectives, principles and policies of the City of Oxnard 2020 General Plan. The City adopted the 2020 General Plan in 1990 to guide development of the City. The Plan includes the following elements: Land Use, Circulation, Public Facilities, Growth Management, Open Space/Conservation, Noise, Economic Development, Community Design, Parks and Recreation, and Housing. A major amendment to the 2020 General Plan relating to open space resources was approved by citizens of Oxnard in November of 1998. This General Plan Amendment established the Save Open Space and Agricultural Resources regulations in the City of Oxnard.

A General Plan Amendment will be adopted concurrently with the adoption of this Specific Plan. It is envisioned that this will establish a “Specific Plan” (SP) land use and appropriate zoning designation(s) over the project area. Thus, the regulations contained in this Specific Plan will govern all future development on the site.

1.3.3 Environmental Impact Report

The Environmental Impact Report (EIR) certified for The Village is the sole environmental document for the Specific Plan and all individual projects which are undertaken pursuant to, and in conformance with, the Specific Plan. The City of Oxnard, as the Lead Agency, prepared the EIR in accordance with the California Environmental Quality Act (Public Resources Code Section 21000 et seq.). Therefore, an Initial Study, Negative Declaration, or other environ-

mental analysis need not be filed for individual site and building plans which are in substantial conformance with the standards and guidelines contained in the Village Specific Plan.

This Specific Plan includes a substantial amount of information governing future build-out of the project area, all of which was considered as part of the Project EIR. Thus, individual development projects which are determined to be in substantial conformance with the Specific Plan may proceed with administrative building design review, and/or if necessary, other discretionary actions without additional environmental review under the California Environmental Quality Act (CEQA). A CEQA exemption or finding of consistency with the Certified Final EIR prepared for The Village Specific Plan would be required prior to review and approval of any subsequent discretionary development projects.

1.3.4 Approval Authority

The City of Oxnard retains authority for approval of the Specific Plan as a whole, amendments to the Specific Plan, and approval of individual development projects undertaken as part of Specific Plan build-out. Approval requests to the City may include, but are not necessarily limited to:

- Approvals related to the Specific Plan as a whole:
 - Approval of the Specific Plan and any amendments to the Specific Plan

CHAPTER 1.0

Introduction

- Tentative Tract Map(s)
 - Development Agreement
 - Owner Participation Agreement
 - General Plan Amendment
 - Zone Change
 - Mobile home closure permit
- Approvals related to individual development projects within the Specific Plan Area:
 - Amendments to the Specific Plan
 - Major and/or Minor Modifications
 - Development Design Review Permits
 - Building and Grading Permits

1.3.5 Applicable Boundaries

Figure 1.3 designates the legal boundaries referred to by the Specific Plan. Those include:

1. *City of Oxnard Boundaries:* The entire 63.3 acres of the project site are located within the City of Oxnard.
2. *Redevelopment Boundaries:* The entire project site is located within the Oxnard Community Development Commission's Historic Enhancement and Revitalization of Oxnard (HERO) Redevelopment Area.
3. *The Village Specific Plan Boundaries:* The boundaries of the entire Specific Plan Area, as shown by Figure 1.3, are as follows:

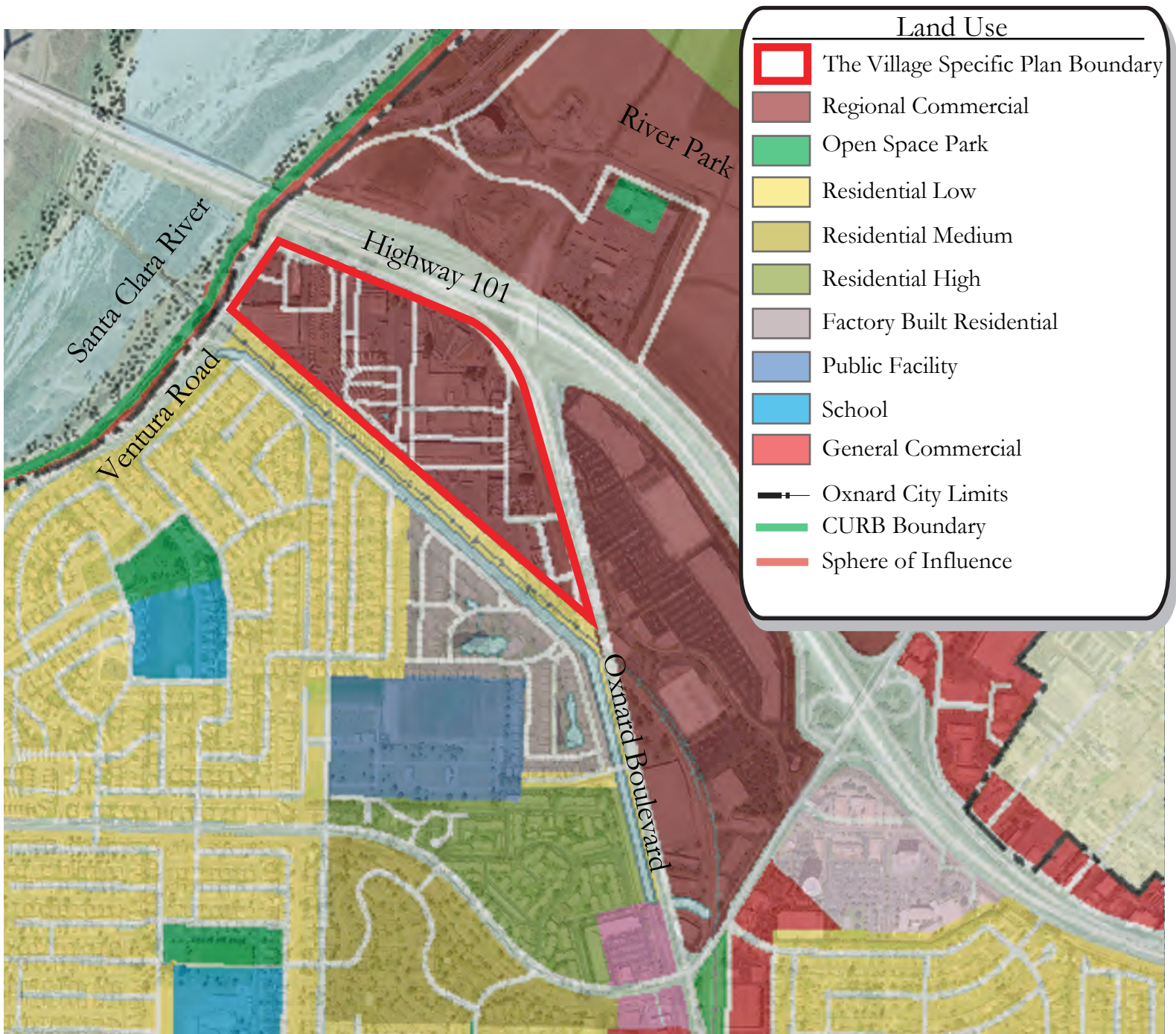
Ventura Freeway (US Route 101 Freeway) and associated landscape area to the north, Union Pacific Railroad to the south, Oxnard Boulevard to the east, and Ventura Road and the Santa Clara River to the west.

1.3.6 Oxnard 2020 General Plan Land Use and Zoning Ordinance Designations

Figure 1.4 illustrates the Oxnard existing 2020 General Plan land use designations for the site. The Specific Plan's existing Land Use Designation is General Commercial. The site has a current zoning designation of General Commercial Planned Development (C-2-PD) and Commercial and Light Manufacturing (C-M). The proposed land use and zoning designation for the Specific Plan area is "Specific Plan" (SP).



Specific Plan Boundary



CHAPTER 1.0

Introduction

1.4 COMMUNITY SETTING

1.4.1 Project Location

The Village Specific Plan is located in the southwest portion of Ventura County, within the City of Oxnard, as shown on Figures 1.2 through 1.4. More specifically, the project site is located near the northwestern edge of the City of Oxnard, and is bordered by Highway 101 to the north, Oxnard Boulevard to the east, the Union Pacific Railroad and El Rio Drain to the south, and North Ventura Road and the Santa Clara River to the west.

1.4.2 Site History

The Wagon Wheel area was originally developed as an industrial/commercial office and shopping center in the 1960s. The property also contains the Wagon Wheel Trailer Lodge, which was constructed in 1953 and includes 171 spaces for both mobile homes and recreational vehicles. Over time, buildings and improvements on the site gradually fell into a state of disrepair, suffering from a combination of functional obsolescence, changing consumer preferences, and the construction of newer shopping facilities, hospitality establishments and industrial parks elsewhere within the immediate trade area. This precipitated private redevelopment of the westerly area into a neighborhood shopping center in 1985. Despite this re-investment, vacancies and deterioration persisted. Then, in 1998, the City established the Historic Enhancement

and Revitalization of Oxnard (HERO) Redevelopment Project and placed Wagon Wheel into a designated redevelopment project area. The objectives of the HERO Redevelopment Project include elimination of blight, economic revitalization, infrastructure improvement, structural rehabilitation, possible hazardous waste cleanup assistance, and other types of assistance for each sub-area within the redevelopment district. This action was followed by redevelopment of the adjacent Esplanade Shopping Center in 2001 and approval of the Riverpark master-planned community in 2002.

Within the same general timeframe, work commenced on a major reconstruction of the Highway 101/Highway 1/Oxnard Boulevard Interchange. This project consisted of the replacement of the existing bridges across the Santa Clara River and widening of the freeway from three to six travel lanes in each direction from Vineyard Avenue in Oxnard to Johnson Drive in Ventura. The highway project was completed in Summer 2007. At present, two off-ramps serve the Wagon Wheel area, one from southbound Highway 101 at Ventura Road and the other from southbound Oxnard Boulevard near the Spur Drive/Wagon Wheel Road intersection. The existing Highway 101/Ventura Road off-ramp was retained while the Oxnard Boulevard off-ramp was replaced with a signalized intersection aligned with Oxnard Boulevard.

CHAPTER 1.0

Introduction

1.4.3 Existing Uses

Formerly known as “Wagon Wheel,” the Specific Plan Area is fully developed with a patchwork of un-planned uses including aging industrial and commercial facilities in the eastern half of the site; a 171-space mobile home park, which currently contains 120 occupied spaces¹ in the center portion of the site; and a neighborhood retail center in the western half, as shown on Figure 1.5. In total, the Wagon Wheel property is comprised of 564,906 square feet of industrial uses, 134,318 square feet of commercial uses and 120 occupied mobile home park spaces. Approximately 170,000 SF of the on-site industrial space and 56,000 SF of the on-site commercial space is currently vacant.

1.4.4 Surrounding Uses

Surrounding land uses include the 702-acre RiverPark Towne Center master-planned community and U.S. Highway 101 to the north, the Esplanade Shopping Center and Financial Plaza to the northeast, the existing Southbank community, the El Rio Drain and the Union Pacific Railroad to the south, and the Santa Clara River to the west, as shown on Figure 1.5.

1.4.5 Circulation

Regional access to the site is provided by the Ventura (U.S. Highway 101)

Freeway. The primary eastern access to the Specific Plan is via Oxnard Boulevard. This roadway has been upgraded to meet the new Caltrans construction designs, including signalized intersections north and south of the 101 Freeway and a four-way signalized intersection at Oxnard Boulevard and the Esplanade Drive /Spur Drive. Additionally, the existing bridge over Oxnard Boulevard linking The Village Specific Plan to the Esplanade Mall is envisioned as an important transportation and pedestrian link to the east. Primary access to the Specific Plan from the west is provided by Ventura Road. This roadway is also undergoing improvements as part of the Oxnard Boulevard Interchange project.

In order to provide a smooth transition from Highway 101 southbound into The Village, the existing Wagon Wheel off-ramp is envisioned as a continuous flow off-ramp with two right-turn lanes and two left-turn lanes at North Ventura Road.

Wagon Wheel Road, which currently traverses the outer portions of the project site, will be abandoned and redirected through the center of the project, creating a new “Main Street;” realignment will provide a convenient automobile and pedestrian /bicycle linkage paralleling Highway 101 between Oxnard Boulevard and North Ventura Road. A regional Class I pedestrian and bicycle trail is proposed along the northern portions of the Specific Plan boundary and along Ventura Road, Oxnard Boulevard, and

¹ As of April 2008, a survey completed by Cabrillo Economic Development Corporation identified 120 out of 171 occupied mobile home spaces.

CHAPTER 1.0

Introduction

the Santa Clara River. These facilities are considered important components of the Specific Plan's overall vision for pedestrian connectivity.

Internal roadways serving The Village include traffic calming devices such as "roundabouts", enhanced pedestrian crossings, on-street parking, tree-lined landscaped parkways, designated off-street bicycle/pedestrian pathways, and comfortable sidewalks. These circulation improvements will effectively slow vehicle traffic down through the site, creating a safer and more enjoyable environment for residents and visitors of The Village.

As the northern gateway into the City of Oxnard, The Village Specific Plan proposes to construct prominent gateway landscaping surrounding the Highway 101/Wagon Wheel Road off-ramp. The signage and landscaping will help to establish a sense of arrival through the combination of a new City of Oxnard gateway monument sign displayed prominently at or near the Wagon Road freeway off-ramp and thematic landscaping features along the Specific Plan area's northern Highway 101 frontage.



Existing Uses

CHAPTER 1.0

Introduction

1.5 PROPOSED LAND USES AND UTILITIES

The Village Specific Plan envisions the mixing of compatible uses, including neighborhood serving commercial, various higher-density residential dwelling types, active and passive recreational amenities, and multi-modal transportation programs. The end result will be a community with a more urban atmosphere. Implementation of the Specific Plan will also offer new employment opportunities, improve on-site and surrounding infrastructure, and will add substantial economic value to the City of Oxnard.

Overall, The Village Specific Plan consists of 1,500 residential dwellings interspersed among a variety of housing types, a mix of office uses and live/work dwellings, neighborhood serving commercial uses, recreational opportunities, and transit uses.

The overall mix of land uses envisioned for The Village Specific Plan are presented in Figure 2.1 Land Use Plan.

1.5.1 Utilities

Currently, 100% of the site is developed with all major sewer, water, electricity, natural gas, and roadway infrastructure. As a result, approximately 95% of the Specific Plan area is impervious. As part of Specific Plan construction, the existing sewer and water infrastructure will be upgraded to fully support the plan's future utility service needs. New and improved drainage and storm water management

infrastructure will also be constructed as part of future development to comply with the current local and statewide Standard Urban Stormwater Mitigation Program (SUSMP) requirements.

1.6 ORGANIZATION OF THE SPECIFIC PLAN

The Specific Plan regulates all aspects of future development through a set of clearly defined Development Standards and Guidelines, a programmatic phasing program, and guidelines for implementation. The intent of these regulations is to ensure a high level of development and design quality.

The location of the standards and organization of the Specific Plan are as follows:

- *Chapter 1: Introduction:* This Chapter outlines the purpose of the Specific Plan and defines the regulatory authority of the Plan and its relationship to the City of Oxnard 2020 General Plan. The Specific Plan's community setting and general development components are also described.
- *Chapter 2: Land Use Plan:* This Chapter establishes the general spatial arrangement of land uses, including the variety and location of proposed land uses, the anticipated number of units and/or square footage within each Planning Area, and a general description of the landscape concept, vehicular circulation network, and oppor-

CHAPTER 1.0

Introduction

tunities for pedestrian and bicycle pathways.

- *Chapter 3: Mixed Use Development Plan:* This Chapter describes and illustrates in detail the mixed use development concept, and the applicable “form-based” development standards and guidelines applicable to the Mixed Use Planning Areas.
- *Chapter 4: Residential Development Plan:* This Chapter describes and illustrates in detail the residential development concepts, and the applicable “form-based” development standards and guidelines applicable to the residential Planning Areas.
- *Chapter 5: Landscape Development Plan:* This Chapter describes and illustrates in the overall landscape design concept, the conceptual designs for the neighborhood parks and public spaces, and the desired streetscape character and associated cross-sections.
- *Chapter 6: Infrastructure Development Plan:* This Chapter describes and illustrates the plans to address on-site and off-site regional infrastructure components, including site demolition phasing, site grading, infrastructure construction, drainage, water quality/storm water management, and parking and transportation.
- *Chapter 7: Architectural Design Standards and Guidelines:* This Chapter describes and illustrates standards and guidelines for architecture and urban design, signage, landscaping, and other development components proposed within The Village Specific Plan area.
- *Chapter 8: Administration and Implementation:* This Chapter defines the policies, programs, and processes that will be adopted to implement The Village Specific Plan. It also defines the roles of the City and Developer during formal design and construction of each particular development phase.
- *Chapter 9: General Plan Consistency Analysis:* This section establishes the Specific Plan’s consistency with the City’s 2020 General Plan Goals and Objectives.



The Village

SPECIFIC PLAN



Chapter 2

Land Use Plan

CHAPTER 2.0

Land Use Plan

2.1 INTRODUCTION

The Land Use Plan provides the basic coordinating elements of this Specific Plan and establishes some of the key development requirements. This Chapter establishes: (1) the proposed land uses for The Village Specific Plan; (2) the general mix, location, size, and total number of residential dwellings, and the size of the commercial uses; (3) the range of permitted uses within the Specific Plan; and (4) the degree of flexibility permitted during construction of the Specific Plan.

2.2 LAND USE CONCEPT

The Village Specific Plan proposes a mix of residential, commercial, recreational, and transit-oriented uses intended to serve a broad range of local and regional users without undermining the surrounding regional commercial market. Figure 2.1 - Land Use Plan and Figure 2.2 Illustrative Site Plan establish the plan's conceptual spatial layout and mix of uses, to which all development standards proposed as part of this Specific Plan apply.

The Village Specific Plan envisions 1,500 total residential dwellings, approximately 50,400 square feet of ground floor commercial retail.

2.2.1 Planning Areas

Areas proposed for development within The Village Specific Plan are differentiated by “Planning Areas” and have been grouped according to form, function, and density. A total of 21 Planning Areas are proposed. They are divided into 7 individual categories, which are intended to allow a variety of uses.

The proposed Planning Area categories include:

- **High Density Residential (H):** (15-30 DU/acre)
- **Very High Density Residential (VH):** (30-70 DU/acre)
- **Mixed Use (MU):** (25-70 DU/acre)
- **High Rise (HR):** (70-100 DU/gross acre)
- **Transit Center Overlay (TCO)**
- **Parks and Open Space (P/OS)**
- **Major Streets**

2.2.2. Location and Characteristics of Planning Areas:

Generally, the residential Planning Areas are spread throughout the Specific Plan in an orientation that creates a logical blend of stand-alone high density multi-family housing; high density mixed-use multi-family housing integrated with the ground floor

neighborhood commercial retail/ office uses; very high density multi-family; and high rise multi-family. Each Planning Area and their representative characteristics are described in detail below.

High Density Residential (H):

The High Density Planning Areas are located in the western area of the Specific Plan. These neighborhoods consist of three-story townhouse and courtyard style building types connected by a network of pedestrian friendly streets. The streets are distinguished by broad sidewalks lined with street trees and landscaped parkways. These dwelling types either front onto a public street or are oriented around a landscaped courtyard. Parking is provided by traditional and tandem two-car garages accessed from an alley.

Please refer to Chapter 4.0 - Residential Development Plan, Chapter 5.0 Landscape Development Plan, and Chapter 7.0 - Design and Architectural Standards and Guidelines for applicable development standards.

Very High Density Residential (VH):

The Very High Density Residential Planning Area was part of the original the Specific Plan approval. As part of the Plan April 2020 Amendment, the area for this land use was removed but the design flats standards for this use remain. To allow this use in the future will require a Specific Plan Amendment and corresponding review of the environmental documents in place.

Please refer to Chapter 4.0 - Residential Development Plan, Chapter 5.0. Landscape Development Plan, and Chapter 7.0 - Design and Architectural Standards and Guidelines for applicable development standards

CHAPTER 2.0

Land Use Plan

Mixed Use (MU):

The Mixed Use Planning Areas are envisioned as the Specific Plan's urban core. The multi-family buildings are multi-story and front onto the vibrant and pedestrian friendly "Main Street" the neighborhood streets, and internal landscaped courtyards. These building types include a mix of two or three levels of residential dwellings above ground floor retail and stacked residential dwellings up to four stories. Parking is provided in a mixture of subterranean or on-grade structures and on and off-street surface parking. The subterranean parking is planned to extend down two levels; on-grade parking structures are planned to include four levels.

Please refer to Chapter 3.0 - Mixed Use Development Plan, Chapter 5.0 Landscape Development Plan, and Chapter 7.0 - Design and Architectural Standards and Guidelines for applicable development standards.

High Rise (HR): The High Rise Planning Areas are located in the northeastern portion of the Specific Plan, north of the Village Green and the southeastern portion of the Specific Plan adjacent to Oxnard Boulevard. The northern High Rise Planning Area envisions development of two high-rise buildings; the southeastern High Rise Planning Area envisions development of a single high rise building. All three high rises can include up to 25 floors (not including subterranean parking). Parking is provided in partially subterranean parking structures planned to include four stories above grade and up to two subterranean levels.

Please refer to Chapter 4.0 - Residential Development Plan, Chapter 5.0 Landscape Development Plan, and Chapter 7.0 - Design and Architectural Standards and Guidelines for applicable development standards.

Transit Center Overlay (ICO)

The Transit Center Overlay is strategically located in the southeast portion of the Specific Plan, adjacent to the existing railroad line utilized by Metrolink and Union Pacific Railroad. This Overlay Area envisions the establishment of transit-oriented uses, making up the Wagon Wheel Sub-Transportation Center, and state-of-the-art multi-modal transportation services. Building and landscape designs are required to celebrate the western style architecture of the existing "Wagon Wheel" Motel.

The Wagon Wheel Sub-transportation Center is envisioned as part of a larger Northern Oxnard Transportation Demand Management Program, which plans to provide residents, commuters, business owners, and their employees with direct access to bus and rail service electric cars, car sharing, services, and vanpools.

CHAPTER 2.0

Land Use Plan

Transit services envisioned within the sub-transportation center may include:

1. Gold Coast Transit and potentially VISTA bus services;
2. At least 50 park and ride spaces; 3. Metrolink shuttle service to and from the City's Transportation Center;
3. Mobility center with access to rental of Personal Electric Vehicles (PEV), Segways, bicycles and car sharing;
4. Commuter vanpools to regional employment centers; and
5. Potential for future Metrolink stop.

Please refer to Chapter 3.0 - Mixed Use Development Plan, Chapter 5.0- Landscape Development Plan, and Chapter 7.0- Design and Architectural Standards and Guidelines for applicable development standards.

Parks and Open Space (P/OS):

The Parks and Open Space Planning Areas are designed to provide a variety of recreational opportunities primarily for residents of The Village. Well landscaped gathering places within the Mixed Use Planning Areas will be developed for the general public. The parks, streetscapes, corridors, and public spaces envisioned within The Village Specific Plan include formal greens and recreation centers, community gardens,

landscaped court- yards and plazas, designated pedestrian and bicycle paths, and gateway and perimeter landscaping. Please refer to Chapter 5.0 for further discussions of landscaping and park designs, and more detailed descriptions of public and private recreation areas.

The Specific Plan's parks and recreation areas vary in size and function, and thus provide a range of amenities, including formal recreation centers, active children's play areas, pool terraces, active and passive turf areas, passive gathering

places, and pedestrian and bicycle pathways. The pedestrian pathways and corridors are designed to create opportunities for active and safe recreation while at the same time interconnecting the proposed residential neighborhoods with the neighborhood commercial services.

The landscape environments envisioned for the perimeter of The Village Specific Plan include prominent monument signage designed to identify one's entry into the City of Oxnard and a green buffer between the US 101 Freeway, Oxnard Boulevard, Ventura Road, and The Village Specific Plan. The perimeter landscaping is planned partly within privately owned property and partly within the shoulder of the US 101 Freeway, which forms the Wagon Wheel property's northern border. The landscape buffer will significantly improve aesthetics along the projects 101 Freeway frontage, and buffer the project from the 101 Freeway, the Union Pacific Railroad line, and neighboring communities.





CHAPTER 2.0

Land Use Plan

2.2.3 Vehicle Circulation

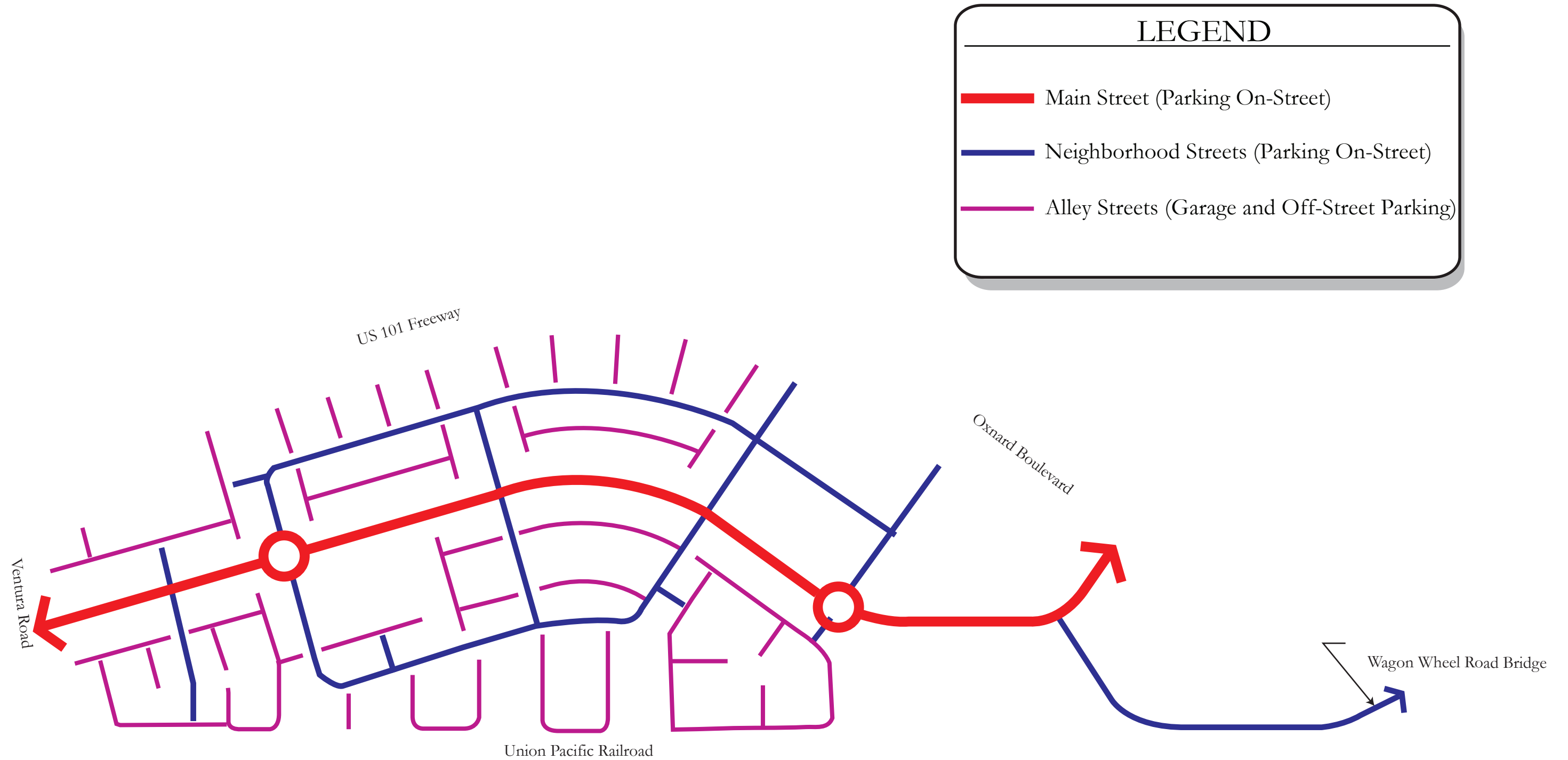
The Village Specific Plan roadway system balances the needs of pedestrians, bicyclists, and automobiles. Oxnard Boulevard, Ventura Road and U.S. Highway 101 will provide regional vehicle access to The Village Specific Plan. The existing intersection of Spur Drive and Oxnard Boulevard will remain the main entrance to the Specific Plan, as it was configured to meet Caltrans intersection standards for a four-way signalized intersection as part of the Oxnard Boulevard Interchange project. Wagon Wheel Road and all other existing roadways within the Specific Plan boundary will be redesigned to create an efficient interconnected network of streets.

From the project entrance, Main Street is planned to meander through the center of the Mixed Use and the High Density Residential Planning Areas. This curved roadway is designed to provide a comfortable environment for pedestrians while slowing traffic. The street will function as an actual public space to be enjoyed by all, and not just a vehicular thoroughfare. The section of Main Street, within the Mixed-Use Planning Areas, is similar in form, function, and aesthetic quality to those found in downtown Oxnard, with one vehicle travel lane in each direction, angled parking, enhanced streetscape paving, traffic calming mechanisms, and wide pedestrian friendly sidewalks.

Continuing west along Main Street to the Live/Work and High Density Planning Areas, the configuration of Main Street transitions from a more urban streetscape design to streetscape with a more suburban design. At this location, Main Street includes landscaped medians, one vehicle travel lane in each direction, striped Class II bike lanes, parallel vehicle parking, landscaped parkways, and sidewalks. Main Street continues in this configuration throughout the western portions of the Specific Plan. Neighborhood streets intersect with Main Street, forming a modified grid to provide multiple points of access to each residential neighborhood. Alleys provide direct access to rear loaded garages within the High Density Residential and Live/Work Planning Areas.

Two roundabouts, on-street parking, enhanced paving for pedestrian crossings, tree-lined parkways, and sidewalks of ample width are planned at strategic locations along Main Street to calm traffic and to promote walking and cycling.

The vehicle speed limit planned for all roadways is 30 mph or less. Reduced vehicle speeds will increase safety within the Specific Plan and permit the unrestricted use of PEVs on the street network as an alternative to automobile usage. See Figure 2.3 for the spatial arrangement of streets within The Village Specific Plan.



Vehicle Circulation Concept

CHAPTER 2.0

Land Use Plan

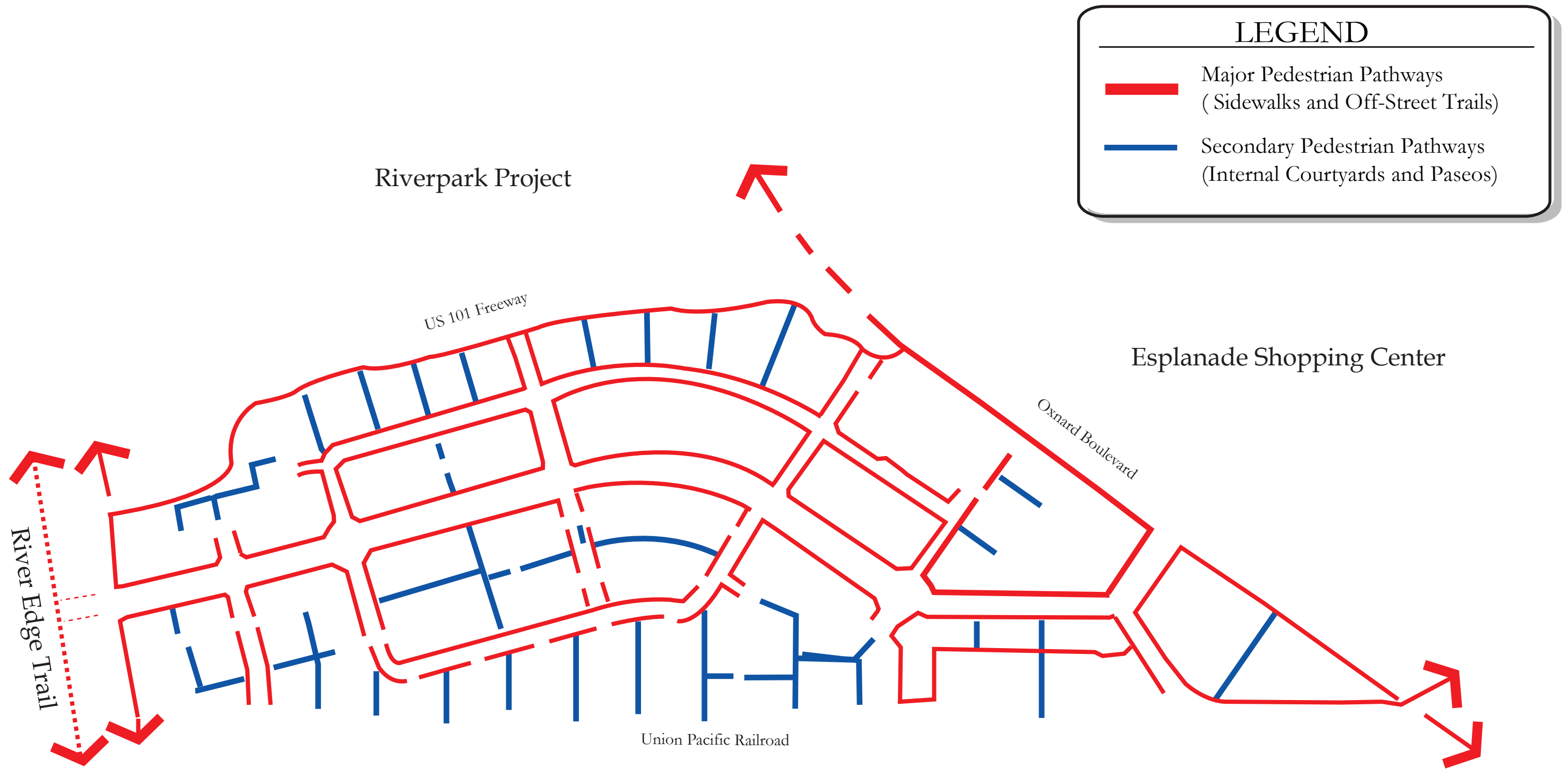
2.2.4 Pedestrian Circulation

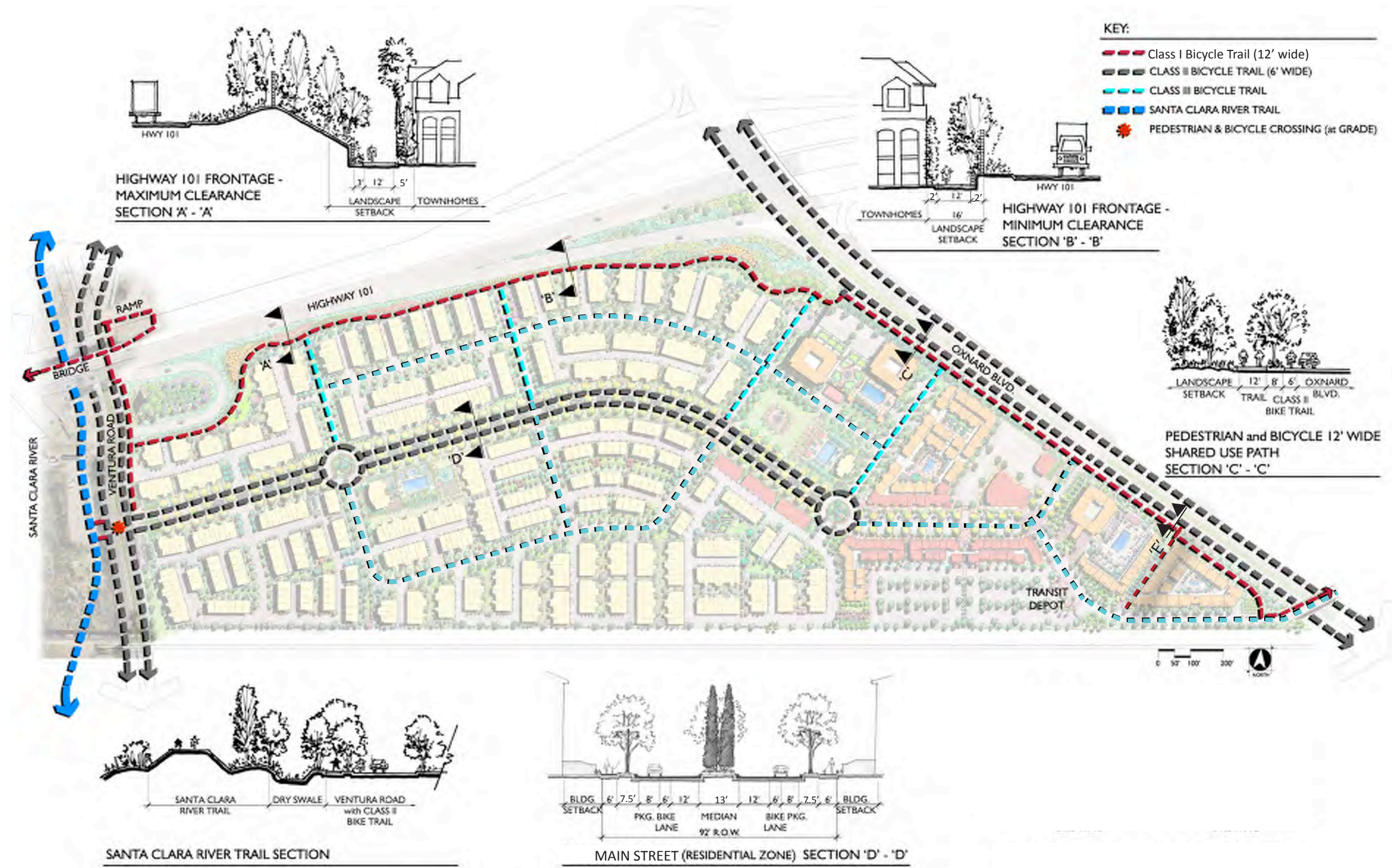
Off-street pedestrian pathways, tree-lined sidewalks, landscaped corridors and public gathering spaces are proposed throughout the Specific Plan to encourage walking as an alternative to short auto trips. Figure 2.4 illustrates the planned pedestrian circulation system. Paramount to the village concept is the safety of pedestrians, which is established via a dedicated Class I bicycle/pedestrian pathway along the northern edge of the Specific Plan area, and a well landscaped pedestrian pathway and sidewalk network which links the proposed residential neighborhoods with the commercial services, the recreational amenities, and the Wagon Wheel Sub-transportation Center.

2.2.5 Bicycle Circulation

The Village Specific Plan will provide a regional bicycle linkage between major transportation corridors and the variety of uses present within the northern Oxnard area. A Class I bicycle pathway is proposed along the west side of Oxnard Boulevard, the northern edge of the Specific Plan, and the western and eastern sides of Ventura Road which will connect Oxnard Boulevard and Ventura Road with the City's future River Edge Trail and the Riverpark master planned community.

Class 2 bike lanes are planned along the residential portions of Main Street, Oxnard Boulevard, and Ventura Road. Class III bicycle lanes are proposed along the neighborhood streets and the commercial portions of Main Street. The location of planned bicycle pathways and connection points are shown in Figure 2.5.





CHAPTER 2.0

Land Use Plan

2.2.6 Vehicle Parking

On-site parking for residents, commercial tenants, business owners and shoppers will be provided by a carefully planned mix of traditional and tandem two-car garage parking, structure parking, on-street parallel parking, on-street angled parking, and off-street surface parking areas.

The structure and surface parking areas within the Mixed Use, Very High Density and High Rise Planning Areas are designed as “Park Once” or shared parking areas. Here, mixing uses reduces parking demand, which allows shoppers to complete multiple tasks without using multiple parking spaces. This approach also reduces the dependence on the automobile, and reduces spillover parking into the residential neighborhoods. Within the “Park Once” environment, adequate parking ratios for the commercial uses range from 1.5 to 2.5 parking spaces per 1,000 square feet. A parking ratio of 1.5 parking spaces/unit for owner occupied condominiums and 1.2 spaces per unit for apartments is required for the parking structures and surface parking supporting the Mixed Use, Very High Density and High Rise Planning areas. Reduced parking ratios can only be used if the City of Oxnard approves a formal Parking Management Study which confirms the ability to reduce parking ratios without creating parking spillover.

Parking for the High Density Residential and Live/Work dwellings is

proposed in alley loaded garages and in on and off-street guest parking areas at a parking ratio of 2.5 spaces per unit. Although parking shortages are not anticipated within the High Density Residential Planning Areas, if the City of Oxnard identifies a parking problem, then it may choose to implement a residential permit parking program. Please refer to Table 2.1 - Parking Summary and Chapter 6, Section 6.9 for expanded discussion addressing vehicle parking.

Table 2.1 - Parking Summary

Planning Areas	Parking Type*	Shared Parking
1 through 14	Attached 2-Car Garage	No
16	Structure	Yes
17	Structure	Yes
18	Structure or Surface	Yes
19	Structure or Surface	Yes
20	Structure	Yes
21	Structure	Yes

*Additional street parking provided on Main Street and Neighborhood Streets

2.2.7 Affordable Housing

The Village Specific Plan is located entirely within the boundary of the City of Oxnard’s Historic Enhancement and Revitalization of Oxnard (HERO) Redevelopment Project Area. Within the redevelopment project area, the City of Oxnard requires that at least 15 percent of all new dwelling units within the project be dedicated to affordable housing. To comply with this requirement, The Village Specific Plan is subject to the requirement of providing two hundred twenty-five (225) rental affordable units which must remain

CHAPTER 2.0

Land Use Plan

affordable for at least 55 years, one hundred nineteen (119) of which must be available to Wagon Wheel Mobile Home Park residents/families. Of the one hundred nineteen (119) units to be made available to Wagon Wheel Mobile Home Park residents/families, ninety (90) shall be made available to and occupied by very low income households (persons and families whose gross incomes do not exceed fifty percent (50%) of the area median income adjusted for size), thirteen (13) shall be made available to and occupied by lower income households (persons and families whose gross income do not exceed sixty percent (60%) of the area median income adjusted for size), ten (10) shall be made available to and occupied by low income households (persons and families whose gross incomes do not exceed eighty percent (80%) of the area median income adjusted for size), and six (6) shall be made available to and occupied by moderate income households (persons and families whose gross incomes do not exceed one hundred twenty percent (120%) of the area median income adjusted to size).

Prior to issuance of the building permit for the 1st residential unit (market rate or affordable), (1) the owner of the project site shall have entered into an affordable housing loan agreement or owner participation agreement with the Oxnard Community Development Commission agreeable in form, content and substance to the Oxnard Community Development Commission, which includes at the least the following

terms and conditions: (a) the owner of the project site shall execute an agreement containing covenants running the land restricting for at least 55 years the maximum income of tenants and the maximum rents that may be charged to tenants for the 225 affordable units, which agreement shall be recorded against the project site in first priority lien position, (b) a scope of development for the 225 affordable units, and (c) a schedule of performance providing for the timely satisfaction of all conditions precedent to the disbursement of any funds from the Oxnard Community Development Commission and the timely commencement and completion of construction of the 225 affordable units; and (2) the agreement containing covenants running with the land described above shall have been recorded against the project site in the first priority lien position. The final form of any affordable housing loan agreement or owner participation agreement shall be subject to the discretionary approval of the Oxnard Community Development Commission and shall include provisions and attachments customarily included in Oxnard Community Development Commission affordable housing agreements, including, but not limited to, conditions precedent to the disbursement of any funds from the Oxnard Community Development Commission.

2.3 LAND USE CONSISTENCY

All components of this Specific Plan are consistent with the City of Oxnard General Plan. To the extent possible, the Specific Plan used the General Plan Land Use Designation Map as a guide to nestablish the individual Planning Areas illustrated on Figure 2.1. However, the overall land use designation for The Village Specific Plan area reflected on the City’s General Plan Land Use Map is a single land use termed “Specific Plan”.

2.3.1 Planning Areas

The Specific Plan Area is comprised of 21 “Planning Areas” illustrated in Figure 2.1. They include:

- PA-1 Parks/Open Space (P/OS)
- PA-2 through 13: High Density Residential (H);
- PA-14: Parks/Open Space (P/OS)
- PA-15: Parks/Open Space; (P/OS)
- PA-16: High Rise Residential (HR);
- PA-17,18, &19: Mixed Use (MU);
- PA-20: High Rise Residential (HR): and Transit Center Overlay (TO) applies to PA 19, 20 and 21 only;

Each Planning Area establishes specific set of permitted land uses, target densities, square footage, and other development regulations. The Planning Areas are utilized and referred to throughout this Specific Plan as the basic components of development to which all standards, guidelines, and implementation measures apply.

2.4 LAND USE REGULATION**2.4.1 Summary**

Section 2.2 and Figure 2.1 describe the character and location of the proposed Planning Areas within The Village Specific Plan.

2.4.2 Planning Area Specifications

Table 2.2 summarizes the gross acreage, target number of dwellings, target density ranges, and commercial square footages (if applicable) within each Planning Area.

2.4.3 Summary of Permitted Uses

Table 2.4 describes the range of permitted uses envisioned within The Village Specific Plan Permitted Land Uses are entitled uses that, once reviewed by the Development Services Director as part of any formal construction design package, do not generally require discretionary review and approval by the Planning Commission and/or City Council for their inclusion as part of the Specific Plan.

Any proposed uses which are not listed in Table 2.4 may be included within The Village Specific Plan only after administrative approval by the City’s Planning Manager, or if appropriate, by the Oxnard Planning Commission or City Council. For a more complete description of administrative review.

2.5 LAND USE FLEXIBILITY

Given that The Village Specific Plan will be built-out over time, as described in Chapter 8, Administration and Implementation, the following measures give the developer flexibility during final design and construction. The goal of these measures is to insure that each development phase can meet current market demand at the time of construction.

2.5.1 Transfer of Residential Dwellings

Within each Planning Area, the Developer may choose to construct dwellings within the range of permitted target net densities (defined as the number of units divided by the area of each planning area measured from the back of the sidewalk) established in Table 2.2, provided that the resulting total unit count does not exceed 1,500 and the total number of dwelling units for each dwelling type does not exceed the Planning Area's target unit count specified on Tables 2.2 and 2.3, and Figures 2.1 and 2.6.

More specifically, this Specific Plan permits the transfer of residential units between all Planning Areas containing residential and/ or commercial uses. For example, the townhouse dwellings within each High Density Residential Planning Areas could be constructed at a target net density ranging from 15-30 units/ acre but the total could not exceed 679 High Density Residential units.

Any design modifications within particular Planning Area in accordance with the above described dwelling transfer program would be subject to review and approval by the Planning Manager. Please refer to Chapter 8: Administration and Implementation, Section 8.7 for additional discussion of the administrative review approval requirements.

2.5.2 Choice of Frontage Types

The “form-based” nature of this Specific Plan is designed to provide flexibility in the choice of building and frontage types within a given Planning Area, which is often times an essential component of ensuring consistency with current market trends and other construction phasing considerations.

All Planning Areas established by this Specific Plan are regulated by development standards and guidelines designed to permit a range of building types, frontage types, and land uses. Less flexible standards are those that describe the desired treatment of building forms, architectural styles, and landscaping.

2.5.3 Development Options

To address potential fluctuations in the real estate market, an optional development scenario has been developed for the Mixed Use Planning Areas 17,18 and 19, High Rise Planning Area 20, Very High Density Planning Area 21, and the Transit Center Overlay.

CHAPTER 2.0

Land Use Plan

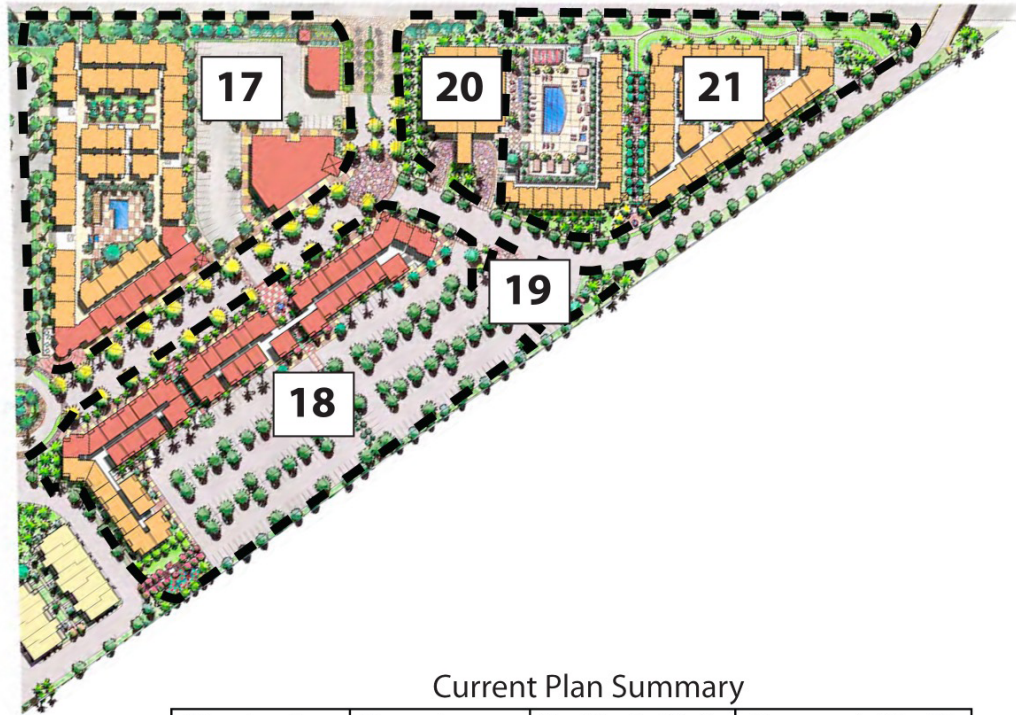
One of two development options may be developed within Planning Areas 17 through 21. They include:

- Current Development Plan: as shown in Figure 2.1, Figure 2.2, and in Table 2.2, Planning Areas 17 through 21 propose up to 503 for-sale residential units and up to 46,400 square of commercial uses with subterranean and surface parking; or
- Development Option A: as shown in Figure 2.6 and Table 2.2 Planning Areas 17 through 21 propose up to 662 rental residential units with slightly modified building configurations. Under this option, all of the multi-family residential units and ground-floor commercial retail spaces are wrapped around on- grade parking structures. Up to 50,400 square feet of commercial space is proposed under this development option. This option removes the 3rd high-rise tower and consolidates Planning Areas 20 and 21. A total of 238 residential units are proposed as part of Planning Area 17: a total of 219 residential units are proposed as part of Planning Area 18, and a total of 205 residential units are proposed as part of the consolidated Planning Area 20. Parking for Planning Areas 17 through 20 can be provided in above ground parking structures or in a combination of on-grade tuck under parking and surface parking as permitted by the City of Oxnard Planning Department. Note that total number of residential units through 20 can be provided in above ground parking structures or in a combination of on-grade tuck under parking and surface parking as permitted by the City of Oxnard Planning Department. Note that total number of residential units

proposed as part of Planning Area 16, High Rise, has been reduced from 304 units to 210 under this development option.

Figure 2.6 provides an illustrative side-by-side comparison of the development options described above. Tables 2.2 provide a tabular comparison

Current Development Plan
Planning Areas: 17, 18, 19, 20, 21



Current Plan Summary

Planning Area	Gross Acreage	Residential Units	Commercial Sq. Ft.
17	3.8	135	16,400
18	4.2	118	30,000
19	0.6	N/A	N/A
20	1.5	138	N/A
21	2.1	112	N/A
Totals	12.2	503	46,400

Option "A"
Planning Areas: 17, 18, 19, 20



Option "A" Summary

Planning Area	Gross Acreage	Residential Units	Commercial Sq. Ft.
17	4.6	238	34,200
18	4.0	219	16,100
19	.55	N/A	N/A
20*	2.5	205	N/A
Totals	11.65	662	50,300

*Note: Option "A" combines Planning Areas 20 and 21 as Very High High Rise Residential, removes 3rd high-rise and reduces unit count from 304 to 205 for High Rise Tower located in PA 16.

CHAPTER 2.0

Land Use Plan

Table 2.2 Land Use Summary

Land Use		Gross Acreage	Target Commercial SF*	Target Unit Count Per Planning Area**
High Density Residential				
	<i>Target Density</i>	<i>(15-30 du/ac)</i>		
		Planning Area 2/3	5	117
		Planning Area 4	4	88
		Planning Area 5	1.9	33
		Planning Area 6	2	33
		Planning Area 7/8	3.6	48
		Planning Area 9	2.2	48
		Planning Area 10	4.4	96
		Planning Area 11	2.5	45
		Planning Area 12/13	4.1	120
		Subtotals	29.7	628
Transit Center Overlay				
		Overlay on planning areas 19,20,21	NA	
Parks and Open Space				
		<i>Parks/Landscape Areas</i>		
		Planning Area 1	2.39	
		Planning Area 14	0.46	
		Planning Area 15	1.52	
		Perimeter/Interior Landscaping	2.22	
		Subtotals	6.59	
Mixed Use Development Option "A"				
	<i>Target Density</i>	<i>(25-70 du/ac)</i>		
		Planning Area 17	4.6	34200
		Planning Area 18	4	16100
		Planning Area 19	0.55	
		Subtotals	9.15	50300
High Rise Residential Development Option "A"				
	<i>Target Density</i>	<i>(70-100 du/ac)</i>		
		Planning Area 16	2.5	210
		Planning Area 20	5.8	205
		Subtotals		415
Other				
		<i>Major Streets</i>	10.5	
		<i>Flood Control / private drives</i>	0.61	
		Subtotals	11.11	
TOTALS FOR SPECIFIC PLAN AREA		62.35	50300	1500
		acres	square feet	dwelling units

*Commercial square footage can be added or subtracted from each of the Mixed Use Planning Areas under the condition that the total commercial area within the Specific Plan does not exceed 50,300 square feet.

**Units can be added or subtracted from each Planning Area under the condition that the total number of units within the Specific Plan does not exceed 1500 and the maximum density for each Planning Area is not exceeded.

Note: This table replaces Tables 2.2 and 2.3. (Option A is shown on this Table 2.2.)

CHAPTER 2.0

Land Use Plan

Table 2.2 Land Use Summary Continued

Land Use	Gross Acreage	Target Commercial SF*	Target Unit Count Per Planning Area**
Other			
Major Streets	12.9		
Subtotals	12.9		
TOTALS FOR SPECIFIC PLAN AREA	63.3	50,400	1500
	acres	square feet	dwelling units

*Commercial square footage can be added or subtracted from each of the Mixed Use and Live/Work Planning Areas under the condition that the total commercial area within the Specific Plan does not exceed 50,400 square feet

** Units can be added or subtracted from each Planning Area under the condition that the total number of units within the Specific Plan does not exceed 1500, the maximum density for each Planning Area is not exceeded, and the total number proposed for each building toe is not exceeded

Table 2.3 - Land Use Summary for Development Option "A"

Land Use	Gross Acreage	Target Commercial SF*	Target Unit Count Per Planning Area**
Mixed Use Development Option "A"			
Target Density: (25-70 du/ac)			
Planning Area 17	3.8	34,200	176
Planning Area 18 and 19	4.8	16,000	219
Subtotals	8.6	50,300	395
Very High Density Development Option "A"			
Target Density: (30-70 du/ac)			
Planning Area 20	3.6		205
(Note: PA 20 and 21 are combined)			
Subtotals	3.6		205
High Rise Residential			
Target Density: (70-100 du/ac)			
Planning Area 16	3.3		207
Subtotals			
	15.5	50,300	807
*Note: Under Option "A" affects only Planning Areas listed above. All other Planning Areas remain as prescribed above in Table 2.2. Under Option A, the total commercial square footage equals 50,300; the total number of residential dwellings total 1,500 and the total number of units allocated for High Rise Residential Planning Area 16 is reduced from 304 to 207 units.			
Transit Center Overlay			
	<u>on Planning Areas 19, 20 and 21</u>	No Additional Development Acreage	

CHAPTER 2.0

Land Use Plan

Table 2.4 Summary of Permitted Uses by Land Use Category

PLANNING AREA	ALLOWABLE USES
High Density Residential	<ul style="list-style-type: none">• Multi-Family Residential Dwellings at 15-30 DU Acre• Day care facilities or other civic related uses• Churches
Very High Density Residential	<ul style="list-style-type: none">• Multi-Family Residential Dwellings at 30-70 DU Acre• Day care facilities or other civic related uses• Churches
High Rise Residential	<ul style="list-style-type: none">• Multi-Family Residential Dwellings at 70-100 DU Acre• Day care facilities or other civic related uses• Churches
Mixed Use	<ul style="list-style-type: none">• Multi-Family Residential Dwellings uses at a density of 25-70 DU/acre <p><u>Acceptable office/service uses include, but are not limited to:</u></p> <ul style="list-style-type: none">• Professional and administrative offices, including medical, dental, attorneys, dry cleaner, transportation oriented uses (such as segway rentals, bicycle rentals, etc.) engineers, architects, insurance, real estate agents, churches, day care facilities, and other offices/services not engaged in merchandizing of goods or products• Multi-Level Parking Structures• Churches• Banks; savings and loan associations and other financial institutions;• Pharmacies engaged primarily in the merchandising of pharmaceutical medical supplies; and <p><u>Acceptable retail uses include, but are not limited to:</u></p> <ul style="list-style-type: none">• Antique store• Grocery, Fruit or Vegetable Store• Restaurants (including outdoor dining)*• Apparel, clothing store, or boutique retail store• Art studio or gallery• Nightclub/Bar (limited to non-amplified live music)*• Delicatessens and sandwich shops• Electronics, television, and radio store,

CHAPTER 2.0

Land Use Plan

Table 2.4 Summary of Permitted Uses by Land Use Category Continued

PLANNING AREA	ALLOWABLE USES
Mixed Use (Cont.)	<ul style="list-style-type: none"> • Coffee Shop • Convenience Store • Jewelry store • Sporting Good Store • Boutique Retail Shop • Cafe • Florist • Furniture store • Gift store • Health centers and clubs • Hobby shop • Interior decorating service • Luggage store • Retail stores or businesses not involved in any kind of manufacture • Similar uses as determined acceptable by the Planning Manager for the City of Oxnard
Transit Center Overlay	<ul style="list-style-type: none"> • Transportation Center and associated small convenience retail. <p><u>Acceptable convenience retail uses include, but are not limited to:</u></p> <ul style="list-style-type: none"> • Small coffee store/kiosk • Newsstand • Rental office for personal electrical vehicles, Segways and bicycles • Train or Bus Ticket sales office • Similar uses as determined acceptable by the Planning Manager for the City of Oxnard
Parks and Open Space	<ul style="list-style-type: none"> • Privately Maintained Community Parks, and Pocket parks, • Publicly and/or privately maintained landscaped areas (gateway landscaping, plazas, paseos, and corridors, etc.) • Publicly and/or privately maintained drainage facilities (such as detention basins, infiltration basins, and other similar storm water management facilities) • Similar uses as determined acceptable by the Planning Manager of the City of Oxnard
Major Streets	<ul style="list-style-type: none"> • Publicly and/or privately maintained infrastructure facilities • Publicly and/or privately maintained landscaping • Similar uses as determined acceptable by the Public Works Director of the City of Oxnard

* Note: Uses proposing sale of alcohol are subject to the approval of a Special Use Permit as required by the City of Oxnard. limeshare uses are not permitted as part of this specific Plan and are not permitted uses



The Village

SPECIFIC PLAN



Chapter 3

Mixed Use Development Plan

CHAPTER 3.0

Mixed Use Development Plan

3.1 INTRODUCTION

3.1.1 Purpose

This Chapter describes the development regulations applicable to the Mixed-Use and Transit Center Planning Areas. These “form-based” regulations are intended to facilitate development of the Specific Plan’s vital, mixed-use pedestrian-oriented urban core. Additional development standards are found in Chapter 2: Land Use Plan, Chapter 5: Landscape Development Plan, and Chapter 7: Design and Architectural Standards and Guidelines. All other standards not specifically addressed by this Specific Plan shall be governed by The City of Oxnard Zoning Ordinance and other pertinent sections of the Municipal Code.

3.1.2 Organization of the Mixed Use Development Plan

Section 3.2 describes the overall vision for the Mixed Use Planning Areas. Section 3.3 defines the mandatory urban design standards. Sections 3.4 and 3.5 establish the building and frontage type standards. Compliance with the standards and guidelines listed throughout this Chapter will ensure construction of the desired urban design quality, aesthetic quality, and building form.

3.1.3 Applicability

The following Planning Areas are subject to these regulations:

- Mixed-Use Planning Areas: PA-17 and 18,
- Transit Center Planning Area: PA-19

3.2 DEVELOPMENT CHARACTER

Building types with European architectural styles and architectural styles historically found throughout Oxnard and Ventura County were used as inspiration for the Specific Plan’s Mixed Use higher density village environment.

3.2.1 Mixed Use (MU)

Planning Areas 17 and 18 encompass approximately 8.0 gross acres and are designed as the Specific Plan’s “Main Street” mixed use commercial and residential environment. Within these Planning Areas, vehicle traffic speeds will be calmed to primarily serve the needs of pedestrians and shoppers, and buildings will have a strong relationship to the street.

Building forms are envisioned as a mix of multi-story buildings with two or three stories of apartments above ground floor retail; stacked flat residential buildings up to four stories are located behind buildings fronting Main Street; stand alone commercial block buildings up to two stories are also permitted. Ground floor residential units are envisioned within the mixed use planning areas (i.e. behind buildings fronting Main Street). Building placement shall be at the back of the sidewalks adjacent to Main Street.

CHAPTER 3.0

Mixed Use Development Plan

Buildings facing the interior portions of the block shall front onto landscaped courtyards.

Parking for commercial and residential uses shall be provided through a combination of on-street diagonal parking, off-street surface parking, and structure parking. Shared parking arrangements are proposed between commercial tenants and the residents as part of a “Park Once” parking management strategy.

The most desirable uses for the ground floor commercial retail establishments include, but are not limited to, restaurants, a neighborhood grocery or convenience store, boutique retail shops, personal services, coffee shops, child day care, and professional office. Please refer to Table 2.4 for a more complete list of permitted uses.

Figures 3.1 and 3.2 include images which generally represent the desired mixed-use building types and their associated active environments.

As discussed in Chapter 2, an alternative development plan, Option “A,” is proposed for the Mixed Use Planning Areas. As shown on Figure 2.6, Option “A” proposes modified building configurations and parking facilities for the Mixed Use Planning Areas. Please refer to Table 2.2 and Figure 2.6 for additional information on this alternative development plan.

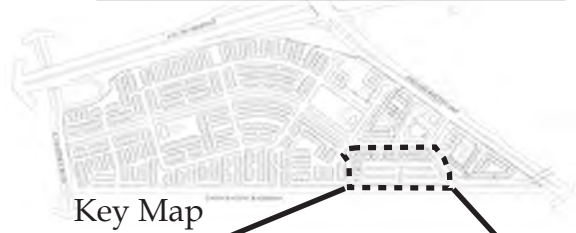
3.2.2 Transit Center (TC)

Planning Area 19 is approximately 0.6 acres in size and is proposed as the Wagon Wheel Sub-Transportation Center. Permitted uses include transit oriented uses and small transit serving convenience uses, such as a small coffee shop and/or a newsstand. Building designs for the Transit Center Planning Area shall emulate the “Old West” architectural style of the Wagon Wheel Motel in a commercial block configuration. Figure 3.3 includes character imagery which represents the desired “western” design elements and building forms.

Planning Area 18 Design Character



Planning Area 18 is envisioned as a continuation of the urban core. Multi-story buildings with 1 or 2-story ground floor commercial uses and 2 or 3 stories of residential dwellings above will continue to frame a pedestrian friendly streetscape which translates into an ideal environment to live, work, and play.



Mixed Use Development Plan

Planning Area 19 (Transit Center) Design Character



Transit Center Design Character

The Transit Center Planning Area of the Village Specific Plan is envisioned as a state-of-the-art, multi-modal transportation center designed with western themed architecture to commemorate the Wagon Wheel Motel. The center will provide access to bus and rail services, as well as digital-age “green” mobility solutions, such as personal electric vehicles, Segways, bicycles, etc.



CHAPTER 3.0

Mixed Use Development Plan

3.3 MANDATORY MIXED USE AND TRANSIT CENTER DEVELOPMENT STANDARDS

The following design elements are considered mandatory for this Specific Plan's Mixed Use Planning Areas:

- **Mixed-Use Commercial Square Footage:** Up to 50,300 square feet of horizontal or vertical mixed use commercial space is permitted within the Mixed Use Planning Areas. The majority of the commercial space shall be incorporated into the Mixed Use Planning Areas on the ground floor of the buildings. Additional commercial space would be subject to review and approval by the City of Oxnard.
- **Mixed-Use Residential:** Densities within the mixed-use planning areas can range from 25-70 units per acre. Building types include two or three stories of residential apartments above ground floor retail oriented along Main Street and residential stacked flats buildings up to four stories located behind building fronting Main Street
- **Walkability:** The Mixed Use Planning Areas shall consist of a high-density pedestrian-oriented "Main Street" development concept, which utilizes high quality architecture, mature landscaping, wide sidewalks, paving enhancements, and street furnishings conducive to outdoor dining and public gatherings. This environment should be an attractive public place where people can live, shop, work, and relax.
- **Identity:** All development within the Mixed Use and Transit Center Planning Areas shall contribute towards the goal of establishing a unique and memorable environment for residents, business owners, and guests of The Village. The following fundamental design elements shall be incorporated into the final building designs:
 - Architectural articulation on all four sides of a building;
 - Street oriented building placement;
 - Pedestrian scale building mass utilizing only high quality materials and architecture;
 - Pedestrian friendly street-scapes with 15 foot minimum and 20 foot maximum width sidewalks capable of providing public gathering spaces, street furniture, out-door dining, and mature landscaping;
 - Street facing public spaces shall be conducive to outdoor dining;
 - Placement of a variety of public art; and

CHAPTER 3.0

Mixed Use Development Plan

- Placement of decorative way finding elements and lighting.
- **Public Spaces:** All outdoor spaces fronting a street within the Mixed Use and Transit Center Planning Areas shall be open to the public and shall have a strong sense of enclosure and a strong inter-relationship between the indoor and outdoor space. These spaces shall provide an interesting and varied array of gathering places and plazas that become the focal point for residents, business owners and their employees, and shoppers. The public spaces shall facilitate social interactions between residents, guests, and patrons, and enhance the quality of life within the on-site neighborhoods and surrounding communities.
- **Design and Construction Quality:** The building architecture, entrances, lighting, signage, parking and overall construction quality within the Mixed Use and Transit Center Planning Areas shall adhere to the highest standards of architecture and urban design all in keeping with European, local, and Western architectural styles.
- **Security and Safety:** Public security and safety shall be given substantial consideration in the final design of buildings, parking areas, and streetscapes. Design elements shall contribute to a safe and secure environment. Thematic signage and way finding, decorative lighting of the streets, low level pedestrian lighting, the lighting of parking areas, the bicycle and pedestrian paths, and building orientation shall all be designed to help establish a safe and secure environment.
- **Parking:** Vehicle parking areas shall be consistent with the “Park Once” concept which enables visitors and residents to park at a convenient location and access a variety of uses in pedestrian friendly environments. The parking areas shall be attractive, well landscaped, effectively screened, well lit, easily accessed, and safe. Angled parking shall be provided along Main Street, and all parking within a structure shall be located behind buildings.
- **Parking Structure Design**
 - Parking structures shall be sited to avoid obstructing the public view and casting shadows on ground-floor pedestrian oriented uses.
 - The maximum allowable height for any above-grade parking structure(s) shall be limited to sixty (60) feet to the top floor level, measured from grade.

CHAPTER 3.0

Mixed Use Development Plan

- Vehicle access to structures shall be set back from the primary pedestrian-oriented frontages.
- Parking structures which primarily serve a building or group of buildings shall be wrapped with mixed use or residential buildings on at least three sides.
- Parking structures that fronting the street shall include appropriate architectural treatments, in the form of a commercial, residential, or civic oriented (such as a recreation terrace or other recreation facility) liner so that the street is appropriately activated.
- Ramps, cars, and sources of artificial lighting shall be minimally visible from public ways and open space.

Some of the mandatory design elements discussed above for the Mixed Use and Transit Center Planning Areas are illustrated in Figure 3.4.



Legend

- ① Gateway Landscaping Accent
- ② Thematic European Architecture - Buildings Face "Main Street)
- ③ Architectural Focal Point - Enhanced Corner Element
- ④ Pedestrian Friendly Streetscapes Enhanced Public Spaces/Outdoor Seating
- ⑤ Diagonal Parking/Shared Surface Parking
- ⑥ Enhanced Paving/Pedstrian Friendly Crossings
- ⑦ Landscaped Buffer
- ⑧ Western Themed Architecture



Mandatory Mixed Use Design Elements

CHAPTER 3.0

Mixed Use Development Plan

3.4 BUILDING TYPES

The buildings within the Mixed Use and Transit Center Planning Areas form The Village Specific Plan’s urban core. This section illustrates the specific building and frontage type requirements for the Mixed Use and Transit Center Planning Areas.

The following building types are permitted within the Mixed Use Planning Areas:

- Building Type 1MU: In-Line Mixed Use
- Building Type 2MU: Residential Wrap or Podium;
- Building Type 1TC: Transit Center; and
- Building Type 1C: Stand-alone Commercial.

The following sections describe the requirements applicable to each building type. Figure 3.5 - Mixed Use Regulating Plan illustrates the conceptual building configurations and type, density, block pattern, and frontage type for the above Planning Areas.



CHAPTER 3.0

Mixed Use Development Plan

3.4.1 Building Type 1MU: In-Line Mixed Use



Illustrative Photo

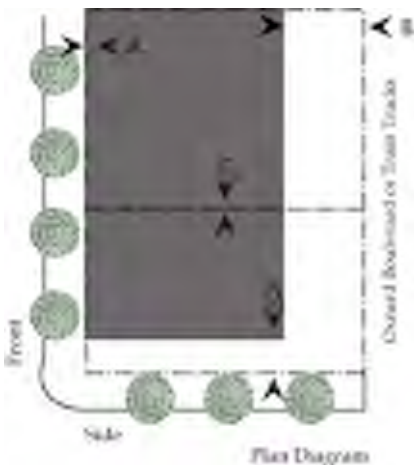
Building Height:

- A. Max: 60', and 4 Stories
- B. Tower element(s) can exceed 60' height limit by 10' (70' max).

Setbacks

Refer to Plan Diagram Below

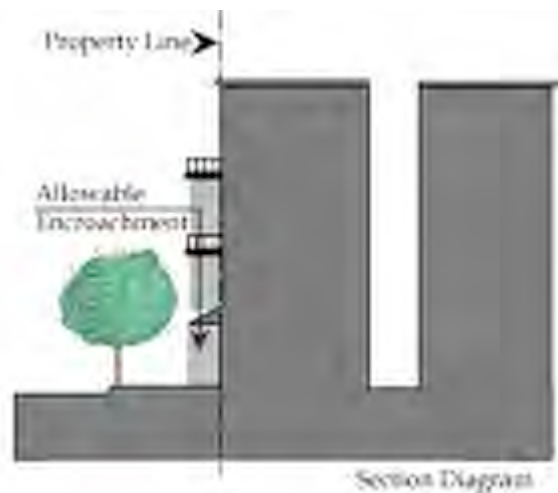
- A. Front: Not Required
- B. Rear: 25' min. from Oxnard Boulevard
- C. Side Yard, Interior: Not Required
- D. Side Street: 5' min.



Permitted Encroachments

Refer to Section Diagram Below

- A. Parapet: 2' max
- B. Canopy/Awning: 5' max
- C. Balconies: 6' max
- D. Patio or outdoor room: To the right of way (R.O.W.)
- E. Chimney: 2' max



Building Orientation and Use

- A. Buildings shall orient towards street, with architectural articulation on all four sides of a building.
- B. For Ground Floor Commercial - The primary building entrance shall be from Main Street.
- C. Parking garage entry shall be set back from Main Street.
- D. Any surface parking lot or parking structure shall be located behind buildings.
- E. Parking structures that fronting the street shall include appropriate architectural treatments, in the form of a commercial, residential, or civic oriented (such as a recreation terrace or other

CHAPTER 3.0

Mixed Use Development Plan

recreation facility) liner so that the street is appropriately activated.

Service Requirements

- A. Service areas such as garbage collection, mechanical equipment, and substations shall not be visible from streets.

- B. Trash storage shall be provided within an enclosed trash storage area within the parking garage or at an accessible off-street location. If the trash storage and/or collection area is located within a sub-surface or on-grade parking garage, the building management company, HOA, or other designated responsible party shall be responsible for moving the trash bins to and from an accessible street side location on trash pick up days. Any off-street trash storage areas shall be fully enclosed and shall be designed with appropriate architectural treatments.

CHAPTER 3.0

Mixed Use Development Plan

3.4.2 Building Type 2MU: Podium or Wrap Mixed Use



Illustrative Photo

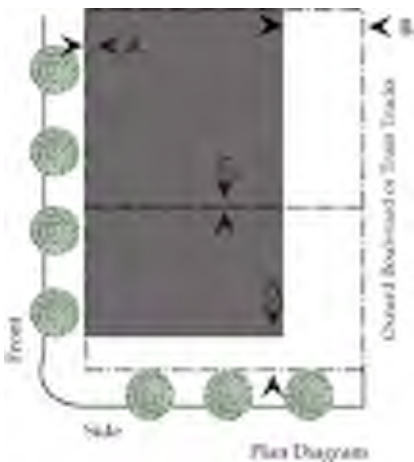
Building Height:

- A. Max: 60', and 4 Stories
- B. Tower element(s) can exceed 60' height limit by 10' (70' max).

Setbacks

Refer to Diagram Below

- A. Front: Not Required
- B. Rear: 25' min. from Oxnard Boulevard
- C. Side Yard, Interior: Not Required
- D. Side Street: 5' min.



Permitted Encroachments

- A. Parapet: 2' max
- B. Canopy/Awning: 5' max
- C. Balconies: 6' max
- D. Patio or outdoor room: To R.O.W.
- E. Chimney: 2' max



Building Orientation and Use

- A. Buildings shall orient towards street or internal courtyard with architectural articulation on all four sides of a building.
- B. Buildings shall wrap or be located directly above parking structure to form effective visual screen parking garage.
- C. Parking garage entry shall be set back from Main Street.
- D. Parking structures that fronting the street shall include appropriate architectural treatments, in the form of a commercial, residential, or civic oriented (such as a recreation terrace or other recreation facility) liner so that the street is appropriately activated.

CHAPTER 3.0

Mixed Use Development Plan

Service Requirements

- A. Service areas such as garbage collection, mechanical equipment, and substations shall not be visible from streets.

- B. Trash storage shall be provided within an enclosed trash storage area within the parking garage or at an accessible off-street location. If a trash storage and/or collection area is located within a sub-surface or on-grade parking garage, the building management company, HOA, or other designated responsible party shall be responsible for moving the trash bins to and from an accessible street side location on trash pick up days. Any off-street trash storage areas shall be fully enclosed and shall be designed with appropriate architectural treatments.

CHAPTER 3.0

Mixed Use Development Plan

3.4.3 Building Type 1TC: Transportation Center



Permitted Encroachments

Refer to Diagram Below

- A. Parapet: 2' max
- B. Canopy/Awning: 5' max
- C. Balconies: 6' max
- D. Patio or outdoor room: To R.O.W.
- E. Chimney: 2' max

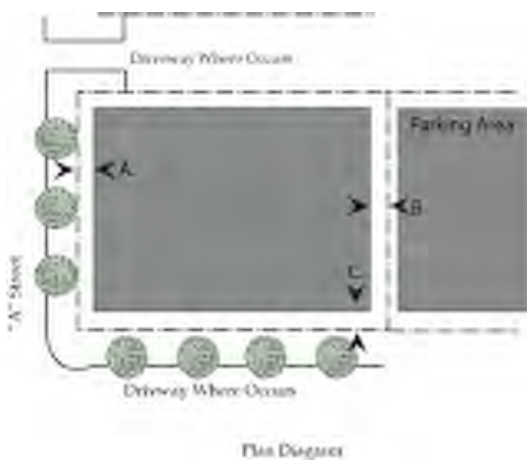
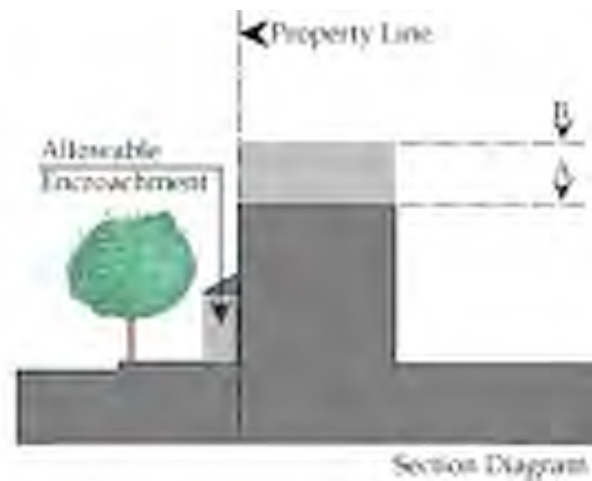
Building Height

- A. Max: 35', and 2 Stories
- B. Tower element(s) can exceed 35' height limit by 10' (45' max.)

Setbacks

See Diagram Below

- A. Front: 5' min.
- B. Rear: 5' min.
- C. Side Yard: 5' min.
- D. Side Street: 5' min.



Building Orientation and Use

- A. Buildings shall orient towards Street "A", with architectural articulation on all four sides of a building.
- B. The primary building entrance shall be from Street "A."
- C. Off-street parking shall be located behind buildings.

Service Requirements

- A. All services shall enter from rear of site.
- B. Service areas such as garbage collection, mechanical equipment, and substations shall not be visible from Street "A".

CHAPTER 3.0

Mixed Use Development Plan

3.4.4 Building Type 1C: Stand-alone Commercial



Illustrative Photo

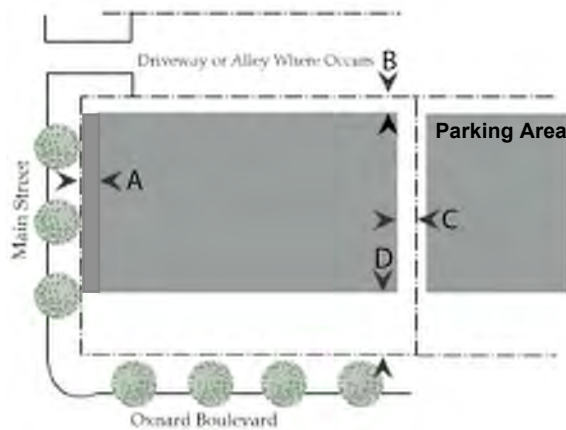
Building Height

- A. Max: 35', and 2 Stories
- B. Tower element can exceed 35' height limit by 10' (45' max.)

Setbacks

See Diagram Below

- A. Front (Main Street): Not Required
- B. Side: 5' min.
- C. Rear: 5' min.
- D. Side (Oxnard Boulevard): 25' min.

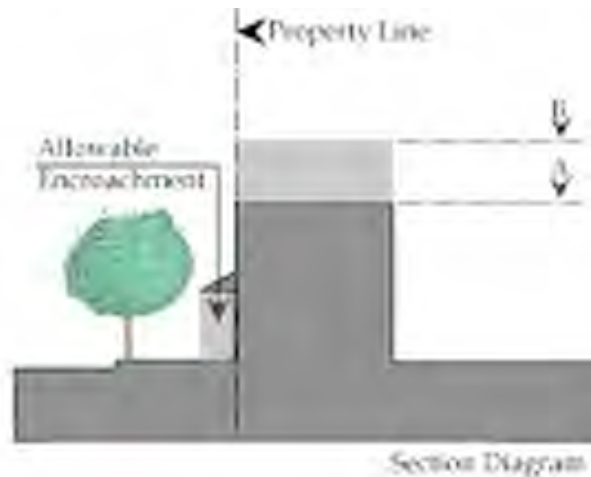


Plan Diagram

Permitted Encroachments

Refer to Diagram Below

- A. Parapet: 2' max
- B. Canopy/Awning: 5' max
- C. Balconies: 6' max
- D. Patio or outdoor room: To R.O.W.
- E. Chimney: 2' max (Above highest point on roof)



Section Diagram

Building Orientation and Use

- A. Buildings shall orient towards Main Street and Oxnard Boulevard, with architectural articulation on all four sides of a building.
- B. The primary building entrance shall be from Main Street. A secondary entrance can be from Oxnard Boulevard.
- C. Off-Street parking shall be located behind buildings

Service Requirements

- A. All services shall enter from rear or side of site
- B. Service areas such as garbage collection, mechanical equipment, and substations shall be appropriately screened from public view.

CHAPTER 3.0

Mixed Use Development Plan

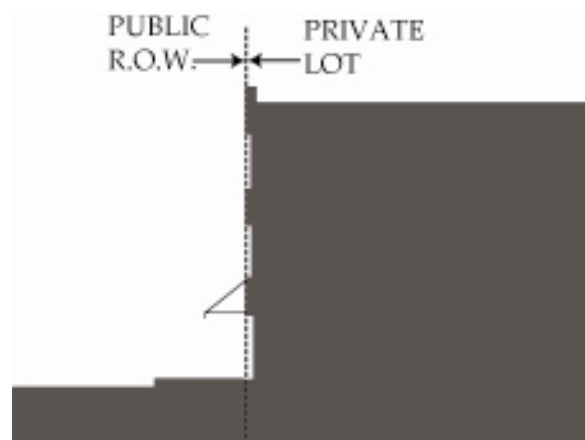
3.5 FRONTAGE TYPES

Public frontages (e.g. the street edge and front yard setback area) create the streetscape character and define the appropriate degree of privacy for the building, and if applicable, its ground-floor uses. Frontages also provide special transitions between interior private space and exterior public space.

This section establishes the range of frontage types permitted within the Mixed Use Planning Areas. All of the frontage types described below are permitted within the Mixed Use Planning Areas.

3.5.1 Storefront

Storefronts will have a frontage where the façade is aligned close to or at the property line with the building entrance directly adjacent to the sidewalk. This is the conventional ground floor retail frontage type, typically associated with substantial windows on the sidewalk level, with an awning positioned above to overlap the sidewalk.



CHAPTER 3.0

Mixed Use Development Plan

3.5.2 Arcade

A frontage where the façade is above a colonnade that overhangs the sidewalk and the public realm extends all the way to the property line. This frontage type is appropriate when retail use is designed as part of the ground floor. The arcade shall be of sufficient depth so that the sidewalk is contained within the arcade, thus discouraging bypass by pedestrians.



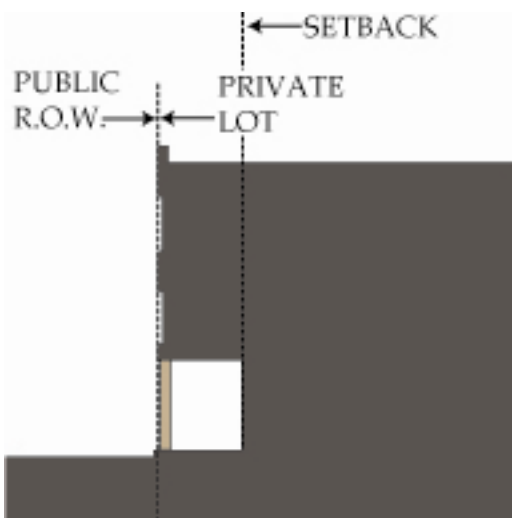
Illustrative Photo

3.5.3 Gallery

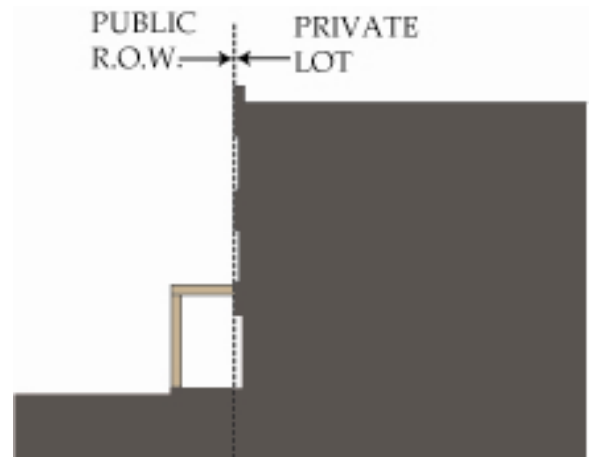
Galleries will have a frontage where the façade is aligned close to the property line with an attached or cantilevered shed or a lightweight colonnade overlapping a sidewalk. This type of frontage is appropriate for retail use at the ground floor. The gallery shall have a depth no less than 10 feet.



Illustrative Photo



Section Diagram



Section Diagram

CHAPTER 3.0

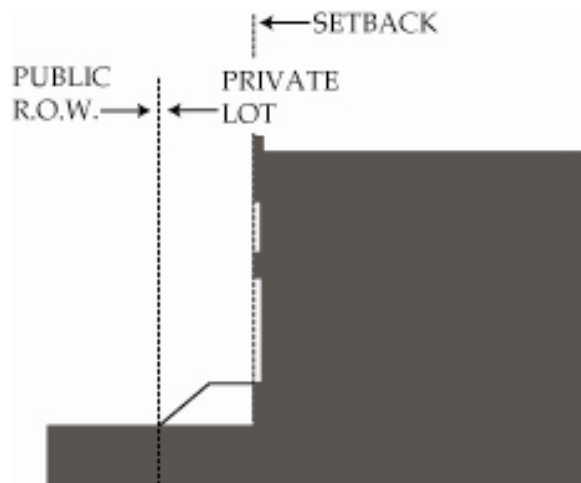
Mixed Use Development Plan

3.5.4 Stoop

Stoops will have a frontage where the façade is aligned close to the property line, with the lower story elevated and setback from the sidewalk sufficient to secure privacy for the windows. The access is usually an exterior stair and landing. This type of frontage is recommended for ground-floor residential uses. The lower floor shall not be higher than 5 feet above the adjacent ground outside.



Illustrative Photo



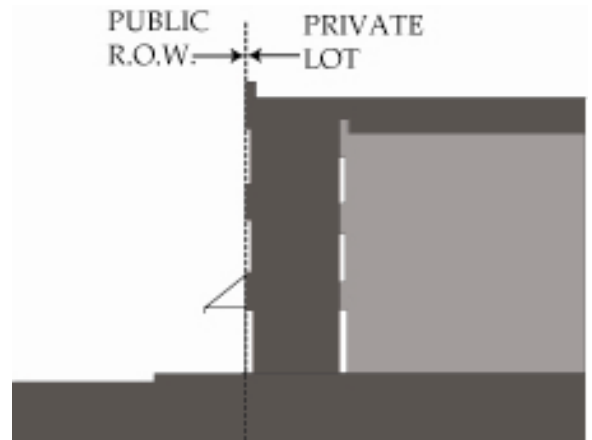
Section Diagram

3.5.5 Forecourt

Forecourts will have a frontage where a portion of the façade is close to or at the property line, while a substantial portion of it is set back. The forecourt created is suitable for gardens and architectural relief. This type should be utilized in conjunction with other frontage types. Trees within the forecourt can overhang the sidewalks.



Illustrative Photo



Section Diagram

CHAPTER 3.0

Mixed Use Development Plan

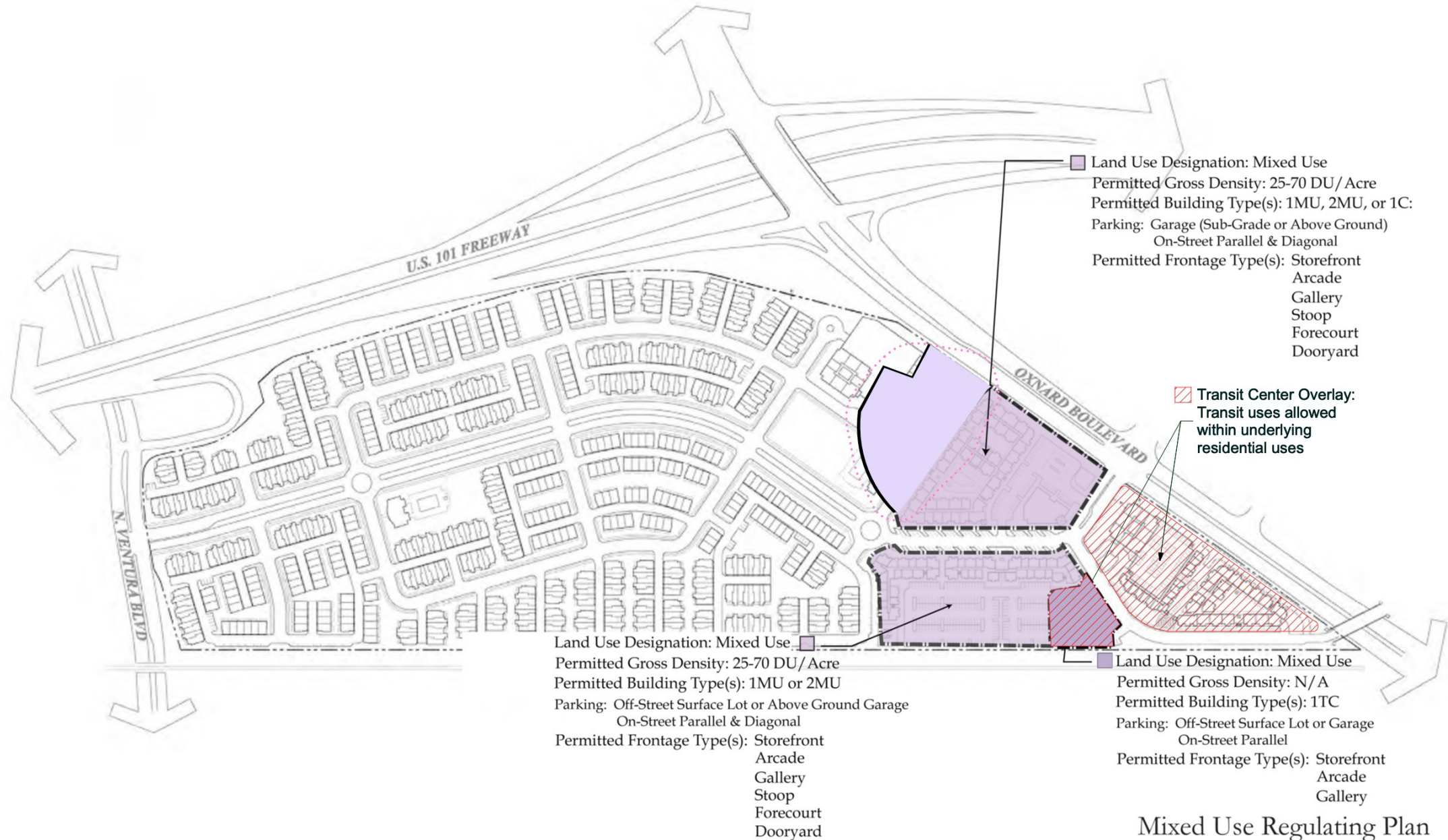
3.5.6 Dooryard

A dooryard frontage is where the façade is set back from the property line by a low garden wall or fence (no more than 2 feet in height), and setback no more than four feet when measured from the sidewalk. This type buffers residential use from urban sidewalks or other urban structures, removing the private yard from the public encroachment.



Illustrative Photo

Section Diagram



Mixed Use Regulating Plan



The Village

SPECIFIC PLAN



Chapter 4

Residential Development Plan

CHAPTER 4.0

Residential Development Plan

The general characteristics of each residential dwelling type are described below.

4.2.1 High Density Residential (H)

The predominant building form within the High Density residential neighborhoods is the “brownstone” style townhouse typically found in larger American cities along the eastern seaboard. These dwelling types front onto public streets, private landscaped courtyards, or private parks.

Vehicle parking for residents of the High Density Residential Planning Areas is provided by an attached two-car garage (traditional and tandem) accessed from an alley. Additional guest parking is provided along Main Street, the surrounding neighborhood streets, and in designated off-street parking areas.

The High Density Residential Planning Areas encompass approximately 28.0 acres and are bordered on the north by the Ventura (101) Freeway, on the west by North Ventura Road, on the south by Union Pacific Railroad and on the east by the Live/Work, Mixed Use and High Rise Residential Planning Areas.

4.2.2 Live/Work (L/W)

The Live/Work Planning Area begins the transition from the High Density Residential Planning Areas to the higher density Mixed Use Planning Areas. A total of 14 Live/Work units front onto Main Street, opposite the Village Green.

The live-work building form is similar to the High Density dwellings; however, each live-work dwelling provides the owner with optional ground floor “flex-space” which can be used as a small commercial office/retail space or additional living space. Up to 4,000 square feet of optional workspace is permitted within the Live/Work Planning Area.

Vehicle parking for residents of the Live-Work dwellings is provided by a traditional two-car garage accessed from an alley. Additional guest parking is provided along Main Street and the surrounding neighborhood streets.

The Live/Work Planning Area encompasses approximately 0.6 acres.

4.2.3 Very High Density Residential (VH)

A courtyard building type is planned for the Very High Density Planning Area. The buildings are up to four stories and include stacked flats and stacked townhouses wrapped around a common courtyard. Parking for residents is provided in two level subterranean parking garages. Parallel parking along Street “A” provides additional guest parking. The buildings front Oxnard Boulevard and Street “A”.

The Very High Density Residential Planning Area encompasses approximately 2.1 acres and is bordered on the east by Oxnard Boulevard, on the west and south by Street “A”, and on the

CHAPTER 4.0

Residential Development Plan

The general characteristics of each residential dwelling type are described below.

4.2.1 High Density Residential (H)

The predominant building form within the High Density residential neighborhoods is the “brownstone” style townhouse typically found in larger American cities along the eastern seaboard. These dwelling types front onto public streets, private landscaped courtyards, or private parks.

Vehicle parking for residents of the High Density Residential Planning Areas is provided by an attached two-car garage (traditional and tandem) accessed from an alley. Additional guest parking is provided along Main Street, the surrounding neighborhood streets, and in designated off-street parking areas.

The High Density Residential Planning Areas encompass approximately 28.0 acres and are bordered on the north by the Ventura (101) Freeway, on the west by North Ventura Road, on the south by Union Pacific Railroad and on the east by the Mixed Use and High Rise Residential Planning Areas.

4.2.2 Section Deleted

4.2.3 Very High Density Residential (VH)

Subject to S.P.A. See 2.2.2.

A courtyard building type is planned for the Very High Density Planning Area. The buildings include stacked flats and stacked townhouses wrapped around common courtyards. Parking for residents is provided in two level subterranean parking garages.

As discussed in Chapter 2, an alternative development plan, Option “A,” has been designed for the Very High Density Planning Area. As shown on Figure 2.6, Option “A” combines High Rise Planning Area 20 and Very High Density Planning

Area 21. This new Planning Area (Planning Area 20) includes three and four-story stacked flats wrapped around an on-grade parking structure and on-street parking areas.

4.2.5 High Rise Residential (HR)

The High Rise Residential Planning Areas are designed to bring a new style of urban living to Oxnard.

Parking for residents, guests, and related service personnel is provided by partially underground and above ground parking structures. Additional guest and service related parallel parking is available along Main Street, Street “B”, Street “C”, and Street “G”.

Planning Area 16 encompasses approximately 3.3 acres; Planning Area 20 encompasses approximately 2.5 acres.

An alternative development plan, Option “A,” has been developed for High Rise Planning Area 20, as described in the Chapter 2. This optional development plan and reduced the number of residential units within Planning Area 16. Please refer to Table 2.2 and Figure 2.6 for additional information on this alternative development plan.

4.3 MANDATORY RESIDENTIAL DESIGN ELEMENTS

The following general design principles and specific development standards are applicable to the development of any residential project as part of this Specific Plan.

4.3.1 The Neighborhood Concept

The “neighborhood concept” within the context of The Village Specific Plan includes the following key elements which shall be used as a guide during the final design and development of each residential dwelling:

4.3.1 The Neighborhood Concept

The “neighborhood concept” within the context of The Village Specific Plan includes the following key elements which shall be used as a guide during the final design and development of each residential dwelling:

- All residential Planning Areas shall be designed as inter- connected neighborhoods;
- The size and configuration of neighborhoods shall ensure that housing, jobs, daily shopping needs and other essential activities are within walking distance of each other;
- As many activities as possible shall be located within walking distance of mass transit;
- The neighborhoods shall contain a diverse mix of housing types to enable residents from a wide range of economic levels, age groups, and household types to live within its boundaries, including residences for-sale and for-rent;
-
- The neighborhoods shall contain an ample supply of publicly and privately maintained open space in the form of squares, plazas, greens, gardens, paseos, and parks whose frequent use is encouraged through their placement and design. The formal neighborhood parks, and pocket parks are proposed to be for the private use of on-site residents, with the exception of The Village Green, which could be a public recreational space. The plazas, squares, and courtyards within the Mixed Use Planning Areas are proposed to be public spaces;
- Publicly and privately maintained spaces shall be designed to encourage the attention and presence of people at reasonable hours of the day and night;
- Neighborhoods shall have well- defined edges

created by lively mixed use areas, variation among streets, and uniquely landscaped environments;

- The neighborhoods shall be connected by pedestrian friendly streets, pedestrian paths, and bicycle paths providing interesting routes to destinations. The streets and paths shall be of appropriate size based upon their intended use, spatially defined by buildings, trees and lighting, and shall discourage high-speed traffic;
- The residential buildings shall be designed with authentic architecture on all sides of the buildings;
- The design features along the fronts of residential structures shall relate the dwelling to the street, park, or landscaped courtyard. Where feasible, the building design shall orient stoops, porches, living rooms, and other more public spaces toward the street; and
- Each neighborhood within the Specific Plan shall have a distinctively European or other compatible architectural style.

4.3.2 Neighborhood Design Standards

The following standards are intended to promote social interaction and pedestrian activity inherently within the design of the residential neighborhoods:

- Buildings shall front towards the streets, landscaped courtyard, or park. Setback standards for each dwelling type are defined in Section 4.4 Building Types.
- All building elevations visible from public view areas (such as parks, courtyards, and public streets) shall be designed with significant architectural elements, including enhanced entries, porches, and building façade variation where appropriate.
- All buildings shall have articulated front elevations. At least two types of articulation are required:
 - 1) Architectural elements shall be provided which extend forward, such as portions of the ground floor which extend towards the street property line beyond the rest of the façade; and
 - 2) Each building façade facing Main Street or a neighborhood street shall be divided horizontally into two or more planes.
- Building clusters shall utilize the streetscape, parks, internal courtyards or plazas as focal points.
- Building corners shall be well defined with vertical elements to help identify the public spaces and improve orientation within the neighborhoods.
- Landscaping and monument signage of appropriate scale and character shall be provided at key intersections or obvious points of transition to identify and/or differentiate the residential neighborhoods.

- Narrower, pedestrian-friendly neighborhood streets and well landscaped pedestrian/bicycle passages shall interconnect the residential neighborhoods with the Mixed Use and Transit Center Planning Areas.
- Alleys proposed to serve the High Density Residential Planning Areas shall be designed to include appropriate turn-around spaces at the terminus of dead-end alleys, or shall be designed in a loop system intersecting with a neighborhood streets when dwellings front onto a landscaped courtyard, or shall be designed as a service alley intersecting with neighborhood streets when dwellings front onto the street.

4.3.3 Parking Structure Design

- Parking structures shall be sited to avoid obstructing the public view and casting shadows on ground-floor pedestrian oriented uses.
- The maximum allowable height for any above-grade parking structure(s) shall be limited to sixty (60) feet to the top floor level, measured from grade.
- Vehicle access to structures shall be set back from the primary pedestrian-oriented frontages.
- Parking structures which primarily serve a building or group of buildings shall be wrapped with mixed use or residential buildings on at least three sides.
- Parking structures that fronting the street shall include appropriate architectural treatments, in the form of a commercial, residential, or civic oriented (such as a recreation terrace or other recreation facility) liner so that the street is appropriately activated.

CHAPTER 4.0

Residential Development Plan

- Ramps, cars, and sources of artificial lighting shall be minimally visible from public ways and open space.

CHAPTER 4.0

Residential Development Plan

- ground-floor pedestrian oriented uses.
- The maximum allowable height for any above-grade parking structure(s) shall be limited to sixty (60) feet to the top floor level, measured from grade.
 - Vehicle access to structures shall be set back from the primary pedestrian-oriented frontages.
 - Parking structures which primarily serve a building or group of buildings shall be wrapped with mixed use or residential buildings on at least three sides.
 - Parking structures that fronting the street shall include appropriate architectural treatments, in the form of a commercial, residential, or civic oriented (such as a recreation terrace or other recreation facility) liner so that the street is appropriately activated.
 - Ramps, cars, and sources of artificial lighting shall be minimally visible from public ways and open space.



Legend

- ① Gateway landscaping accent
- ② Where possible, residential dwellings front onto the public street
- ③ Well landscaped and narrow neighborhood streets promote pedestrian/bicycle use and connect to pedestrian/bicycle paths
- ④ Residential dwellings oriented around specialized open space: plazas, squares, greens, courtyards, and recreation facilities and parks

CHAPTER 4.0

Residential Development Plan

4.4 BUILDING TYPES

The High Density Residential, Live/Work, Very High Density Residential, and High Rise Planning Areas form the Specific Plan’s residential neighborhoods. This section establishes specific building type standards for the residential Planning Areas.



The following building types are permitted within the residential Planning Areas:

Building Type H: High Density Residential

Building Type VH: Very High

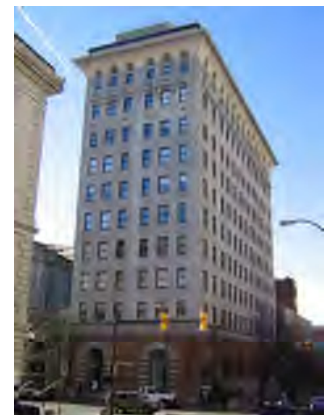
Density Residential

Building Type: HR: High Rise

Building Type ITC: Transportation*



The following sections describe the requirements applicable to each building type. Figure 4.2 -Residential Regulating Plan illustrates the conceptual building configuration and type, density, block pattern, and frontage type for the residential Planning Areas.



* Building Type ITC Transportation can only occur within the Transit Center Overlay

CHAPTER 4.0

Residential Development Plan

4.4.1 Building Type H: High Density Residential



Illustrative Photo

Building Envelope

Lot Width: 15' min.
Building Height: 3-stories and 46' max. to top of parapet

Lot Frontage

Lots front the public street or landscaped courtyard and shall be served by a residential lane at the side or the rear.

Setbacks

Refer to Plan Diagram Below

- A. Front: 5' min. to 10' max.
- B. Rear: 0' min.
- C. Side, Street: 10' min. at corner lots
- D. Side, Alley: 8' min. at corner lots
- E. Side Between Buildings: 10' min.
- F. Front Between Buildings: 25' average, 20' min

Typical High Density Configuration



Concept Rendering

CHAPTER 4.0

Residential Development Plan

Building Orientation and Use

- Buildings types and frontages shall generally be in accordance with the Regulating Plan (Figure 4.2).
- Interior spaces between courtyard oriented building clusters shall include paseos, and pedestrian connections from the front to the rear property lines. Overall paseo width (including landscaping) should be 20' minimum, average 25' with meandering concrete paths and ample landscaping. If a single paseo is provided, it shall be located mid-block.
- Main access to each unit shall be provided directly from the street or landscape courtyard.
- The ground floor shall be designed so that living areas (i.e. living room, family room, dining room, etc.), rather than sleeping or service rooms, are oriented towards fronting street or courtyard.

Open Space

- At least 10% of each lot shall be dedicated towards common open space.

Service Requirements

- Parking and other service uses shall be located within access lanes.

- Access to dry utilities, utility meters and other service functions shall be from the alley.
- A maximum 5 foot planting strip shall be provided in front of each garage to soften its appearance.

Parking Requirements

- Each dwelling unit shall be provided with two enclosed garage parking spaces, served by an access alley.
- Parking along public streets and designated off-street surface parking areas, as shown on Figure 6.4 Parking Plan, shall be provided to satisfy the requirement for visitor parking.
- Parking shall be prohibited within all alleys. Parking can be prohibited by painting the curbs red, no parking painted on the aprons, no parking signs in the alley, and if necessary, recording a deed restriction on each individual lot that prohibits owners from parking in the alleys and would permit enforcement by the HOA and/or City of Oxnard.

Trash Storage and Collection

- Each dwelling unit shall include a 20 square foot trash storage area outside of individual garages. Trash collection shall be from the alleys, but when alleys are not

CHAPTER 4.0

Residential Development Plan

conducive to trash pick up, street side pick up will be required.

- The trash storage area shall be screened from the street and large enough to contain at least two trash containers and shall be located in such a manner that allows container removal without vehicle removal.
- Where individual trash can storage areas are not feasible, common trash bins designed to serve a cluster of residential dwellings, shall be located off-street at a location acceptable to the City of Oxnard Sanitation Department and shall be located within an appropriately designed trash enclosure. A determination of an acceptable refuse service and storage program shall be completed as part of the City's Development Design Review (DDR) process.

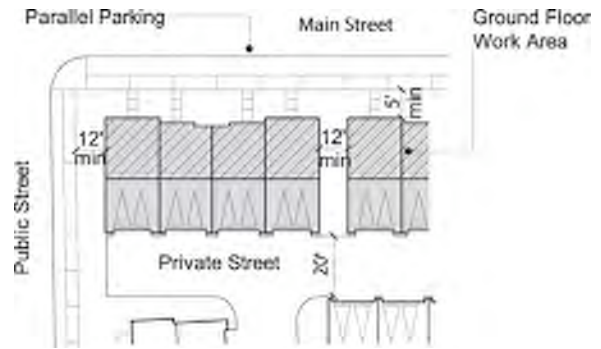
CHAPTER 4.0

Residential Development Plan

4.4.2 Building Type L/W: Live/Work



Illustrative Rendering



Typical Live/Work Configuration

Lot Frontage

Lots front the public street and shall be served by a private lane at the rear.

Building Envelope

Lot Width: 20' min.
Building Height: 3-stories, and 46' max. to top of parapet



Concept Rendering

Setbacks

Front: 5' min. to 10' max.
Rear: 0' min.
Side: 12' min. at corner lots
Side Between Buildings: 12' min.

CHAPTER 4.0

Residential Development Plan

Building Orientation and Use

- Buildings type and frontages shall be in accordance with Regulating Plan (Figure 4.2).
- The main entrance to the ground floor office/flex space shall be from a public street.
- The main entrance to the upper floor residential space shall be from the front of the building. Secondary access can also be provided from the garage.
- Building architecture should complement the pedestrian orientation of the Mixed Use Planning Areas.

Open Space

- At least 10% of each lot shall be dedicated towards common open space.

Service Requirements

- Parking and other service uses shall be accessed via alley.
- Access to dry utility meters and other service functions shall be from the alley.
- A maximum 5 foot wide planting strip shall be provided in front of each garage to soften its appearance.

Parking Requirements

- Each dwelling unit shall be provided with two enclosed garage parking spaces, served by an access alley.
- Parking along public streets and designated off-street surface parking areas, as shown of Figure 6.4 Parking Plan, shall be provided to satisfy the requirement for visitor parking.
- Parking shall be prohibited within all alleys. Parking can be prohibited by painting the curbs red, no parking painted on the aprons, no parking signs in the alley, and if necessary, recording a deed restriction on each individual lot that prohibits owners from parking in the alleys and would permit enforcement by the HOA and/or City of Oxnard.

Trash Storage and Collection

- Each dwelling unit shall include a 20 square foot trash storage area outside of individual garages. Trash collection shall be from the alleys, but when alleys are not conducive to trash pick up, street side pick up will be required.
- The trash storage area shall be screened and large enough to contain at least two trash containers and shall be located in such a manner that allows

CHAPTER 4.0

Residential Development Plan

container removal without vehicle removal.

- Where individual trash can storage areas are not feasible, common trash bins designed to serve a cluster of residential units, shall be located off-street at a location acceptable to the City of Oxnard Sanitation Department and shall be located within an appropriately designed trash enclosure. A determination of an acceptable refuse service and storage program shall be completed as part of the City's Development Design Review (DDR) process.

CHAPTER 4.0

Residential Development Plan

4.4.3 Building Type VH: Very High Density Residential



Illustrative Photo

Lot Frontage

Lots front the public street and are arranged around a common court(s).

Building Envelope

Lot Width: 25' min
Building Height: 60' max to top of parapet or midpoint of roof

Setbacks

Front: 25' min. (from Oxnard Blvd.)
10' min. (from Main St. or "A" St.)
Rear: 10' min.
Side: 10' min.
Side Between Buildings: 25' min. for use as landscape courtyard(s)



Typical Very High Density Configuration



Concept Rendering

CHAPTER 4.0

Residential Development Plan

- The main entrance to each ground floor dwellings shall be directly from the street or courtyard. Access to dwellings on the upper floor shall be from an elevator or corridor. Interior circulation to each dwelling shall be from a corridor.
- Parking garages beneath the buildings or located on grade shall be accessed from Street "A"
- Access shall be provided between the garage and every level of the building.

Open Space

- Landscaped courtyards shall include recreational amenities. Courtyards can be located on the ground or on a podium.

Service Requirements

- Parking for service providers shall be located within the subterranean or surface parking garage or lot or on Street "A".
- Access to dry utility meters and other service functions shall be from the Street "A" and shall be effectively screened.

Parking Requirements

- Each dwelling unit shall be provided with indirect access to two parking spaces within the parking garage or within a combination of tuck under

parking and surface parking sufficiently screened from public view. Surface parking shall be subject to review and approval by the City's Planning Department

- Parking along public streets and designated off-street surface parking areas, as shown of Figure 6.4 Parking Plan, shall be provided to satisfy the requirement for visitor parking and may be shared as part of a Shared Parking Management Program.

Trash Storage and Collection

- Trash storage shall be provided within an enclosed trash storage area within each parking garage or at accessible off-street street locations. If trash storage/collection areas are located within sub-surface garages, the HOA or other designated responsible party shall be responsible for moving the trash bins to and from an accessible street side location on trash pick up days. Any off-street trash storage areas shall be fully enclosed and shall be designed with compatible architectural treatments.
- Internal trash and recycling chutes, or other acceptable means of internal trash disposal shall be provided in a central location within each floor or within each residential unit. Trash from the chutes will be conveyed to a central trash compactor/bin located at the garage level. The Homeowners' Association shall be responsible for maintenance.

CHAPTER 4.0

Residential Development Plan

4.4.4 Building Type HR: High Rise



Lot Frontage

Lots shall front a neighborhood street and shall be served by parking garage entry at the side of each lot.

Building Envelope

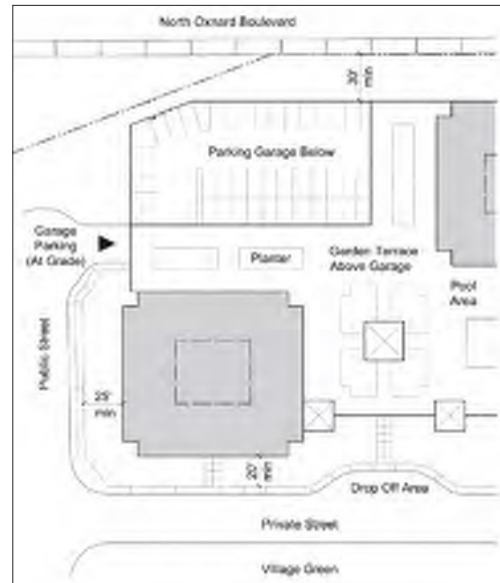
Lot Width: 100 feet min.
Building Height: 100' max

Setbacks - Planning Area 16

Front: 20' min. from "G" Street
Rear: 30' min. from Oxnard Blvd.
Side: 10' min. from 101 Freeway R.O.W.
Between Buildings: 80' min.

Setbacks - Planning Area 20

Front: 20' min. from "A" Street
Side: 10' min. from Main Street
Rear: 25' min. from Oxnard Boulevard



Typical Tower Configuration



Concept Rendering

CHAPTER 4.0

Residential Development Plan

Building Orientation and Use

- Buildings types and frontages shall be in accordance with the Residential Regulating Plan (Figure 4.2).
- The main entrance to the towers shall be from a ground floor lobby fronting the street. Elevator access shall be provided from the subterranean portions of the parking structure and the ground level lobby to all levels of the building.
- The towers shall be offset to provide views from all sides of the buildings.

Service Requirements

- Parking for service uses shall be located on the surrounding public streets.
- Access to the parking garages shall be from one of the streets at the sides of each lot (from Street “A”, Street “B” or Street “C”).
- Access to dry utility meters and other service functions shall be from the streets at the sides of the lot and shall be effectively screened.

Parking Requirements

- Each dwelling unit shall be provided with indirect access to two parking spaces within the parking garage. Note that some of the garage parking spaces may be shared as part of the Parking Management Program.
- Additional garage parking and on- street parallel parking spaces, as shown of Figure 6.4 Parking Plan, shall be provided to satisfy the requirement for visitor parking. Guest parking will also be provided within the Shared Parking District.

Trash Storage and Collection

- Trash storage shall be provided within an enclosed trash storage area within the parking garage. The building management company, or HOA, designated responsible party shall be responsible for moving the trash bins to and from an accessible street side location on trash pick up days.
- Interior trash and recycling disposal areas and / or chutes will be provided in a central location within each floor or within each residential unit. Trash from the chutes will be conveyed to a central trash compactor or bin located at the garage level. The Homeowners’ Association shall be responsible for maintenance and upkeep of the internal trash disposal system.

CHAPTER 4.0

Residential Development Plan

4.5 FRONTAGE TYPES

Public frontages (e.g. the street edge and pedestrian way) and private frontages (the front yard setback area) create the streetscape character and define the appropriate degree of privacy for the building, and if applicable, its ground-floor uses. Frontages also provide transitions between interior private space and exterior public space.

This section establishes the range of frontage types permitted within the High Density, Live/Work, Very High Density, and High Rise Planning Areas. The frontage types permitted within each Planning Area are outlined on Figure 4.2.

4.5.1 Stoop

Stoop is a frontage type where the façade is aligned close to the property line, with the lower story elevated from the sidewalk sufficient to secure privacy for the windows. The access is usually an exterior stair and landing. This frontage type is recommended for ground-floor residential uses. The lower floor shall not be higher than 3 feet above the adjacent ground outside.



Illustrative Photo

Section Diagram

CHAPTER 4.0

Residential Development Plan

4.5.2 Forecourt

Forecourt is a frontage type where a portion of the façade is close to or at the property line, while a substantial portion of it is set back. The forecourt created is suitable for gardens and architectural relief. This frontage type should be utilized in conjunction with other frontage types. Trees within the forecourt can overhang the sidewalks.



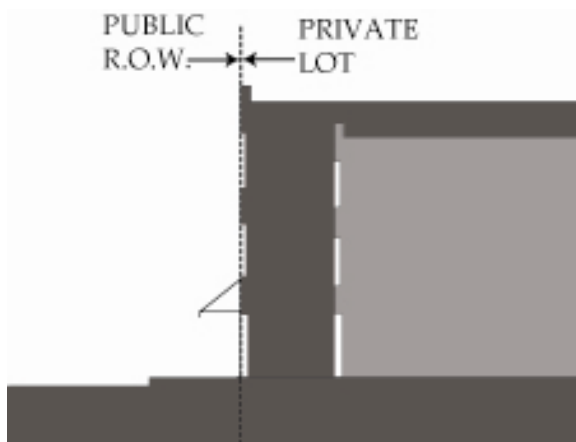
Illustrative Photo

4.5.3 Dooryard

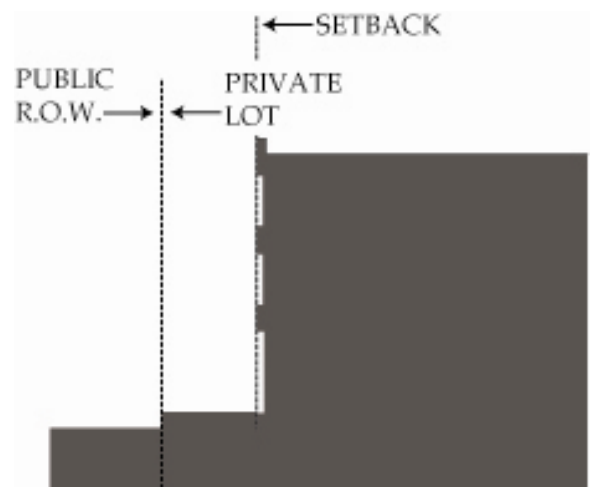
A dooryard frontage is where the façade is set back from the property line by a low garden wall or fence (no more than 2 feet in height), and setback no more than four feet when measured from the back of the sidewalk. This type buffers residential use from urban sidewalks or other urban structures, removing the private yard from the public encroachment.



Illustrative Photo



Section Diagram



Section Diagram

CHAPTER 4.0

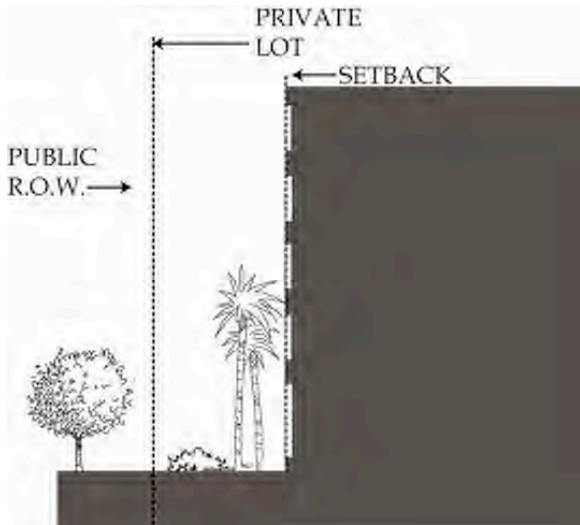
Residential Development Plan

4.5.3 Common Yard

Common yard is a frontage type where the façade is set back at least 15 feet from the property line. The yard created can be fenced or unfenced and is visually continuous in landscaping.

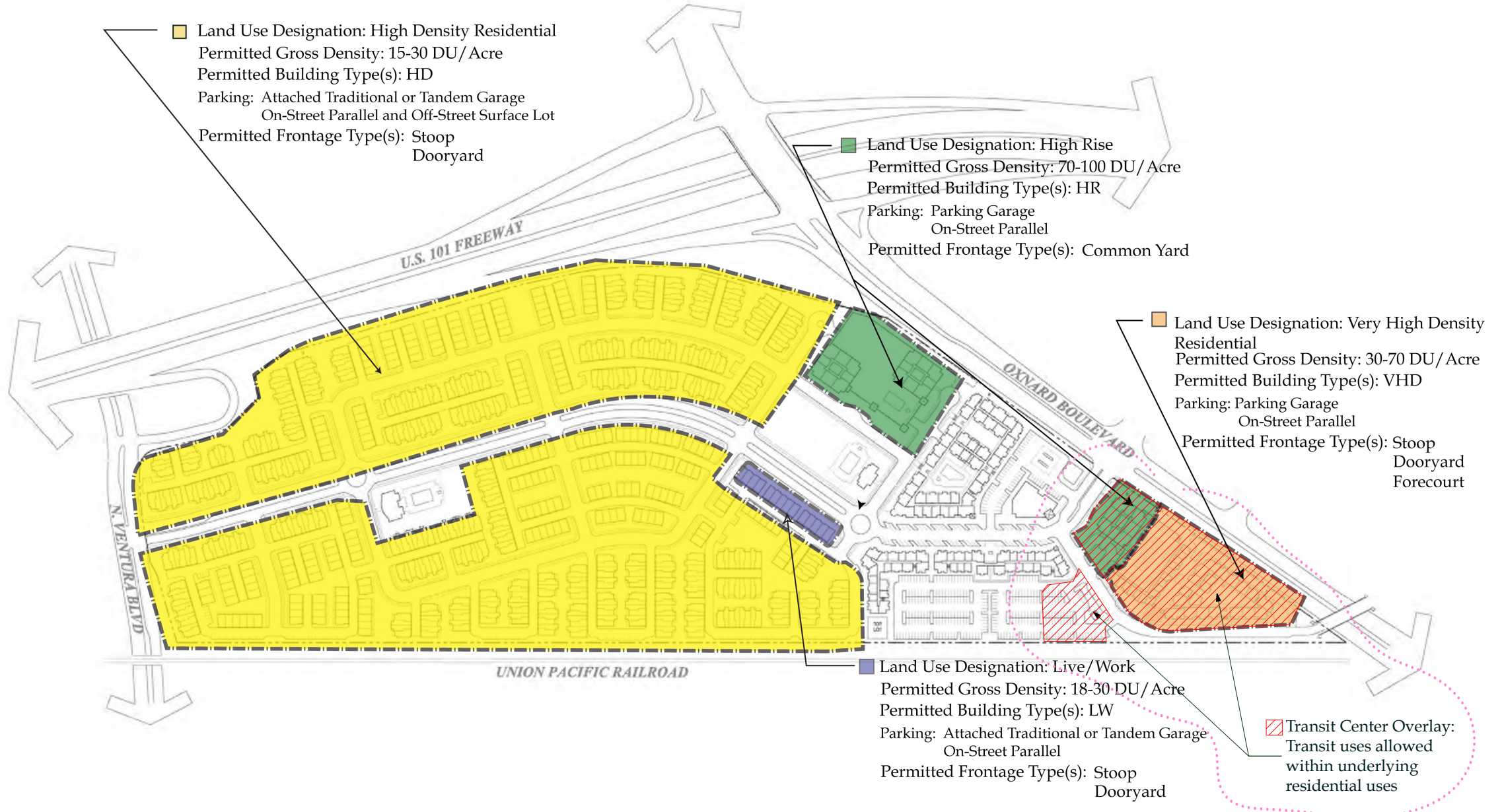


Illustrative Photo



Section Diagram

Residential Development Plan



Residential Regulating Plan



The Village

SPECIFIC PLAN



Chapter 5

Landscape Development Plan

5.1 INTRODUCTION

Landscaping is an especially important component of The Village Specific Plan. Unique environments within The Village Specific Plan will be defined as much by their landscaping as by their architecture and urban design.

This Chapter establishes the landscape design concepts for The Village Specific Plan as well as the guidelines and standards from which formal landscape plans will be developed and maintained by both the developer and/or the City of Oxnard.

Section 5.2 defines the mandatory design concepts applicable to all landscape designs in The Village Specific Plan. Section 5.3 describes the streetscape design concepts, which plays an integral role in establishing the desired pedestrian-oriented character of The Village Specific Plan. Section 5.4 describes the conceptual designs for the recreation areas and pedestrian pathway network. Section 5.5 describes the landscaping concept proposed for the main entrances and the critical edges fronting the U.S. 101 Freeway, Ventura Road, Oxnard Boulevard, and the Union Pacific Railroad. Section 5.6 describes the opportunities to combine landscaping with natural storm water management solutions throughout the Specific Pan. Section 5.7 provides a list of preferred plant materials throughout the Specific Plan.

The design concepts and principles discussed in this chapter are subject to

refinement as the formal architectural and engineering plans are developed and submitted to the City for review during the City's design review and plan check processes. However, the basic landscape concepts and level of quality, as described in this Chapter, shall be adhered to as part of any formal development plan submittal. Any landscape standards not addressed by this Specific Plan shall be subject to the City of Oxnard's existing landscaping standards.

5.2 MANDATORY LANDSCAPE DESIGN PRINCIPLES

The streetscapes, open spaces, and parks within the Specific Plan are "place making" opportunities. To achieve the desired standard of quality and sense of place, landscaped areas shall be given a unique character and image which collectively reinforces the identity of the public and/or private realm. Figure 5.1 illustrates the preliminary opportunities for landscaping within the Specific Plan, and identifies the maintenance responsibilities for each landscape area.

As shown in Figures 5.1, there are ample opportunities for landscaping as part the Specific Plan. The following fundamental principles shall guide the formal landscape designs:

- ***Provide a diverse range of open spaces:*** An assortment of parks and opens spaces are programmed to address neighborhood recreational needs. The park space shall enhance neighbor-







hood identity and provide a range of passive and active options for recreation.

- ***Provide plant materials and landscape design features which create long term sustainability:*** Landscaping shall include predominantly drought tolerant native or naturalized plant materials with proven adaptation to the region's Mediterranean climate, and plant species capable of providing storm water filtration where appropriate.
- ***Create a sense of place:*** Street rights-of-ways, sidewalks, plazas, parks, courtyards, greens, and landscaped paseos between residential neighborhoods and commercial areas shall be an important component of The Village open space network designed to create neighborhood identity, enhance the enjoyment of the community through recreation, and orient people within the development.
- ***Provide linkages to regional open space and trails:*** The Village Specific Plan's streets, sidewalks, paseos, and pedestrian/bicycle trails shall be linked to the Santa Clara River Regional Trail and other existing and planned pedestrian/bicycle trails within the northern Oxnard area.
- ***Provide "Gateway" Landscaping:*** The Village Specific Plan area is

located at the entrance to the City of Oxnard. Given the prominent views into The Village Specific Plan area from the 101 Freeway, special attention shall be given to edges of the property along the 101 Freeway, Ventura Road, Oxnard Boulevard, and Union Pacific railroad. Edge landscaping within The Village Specific Plan shall establish an attractive mix of "gateway" landscaping and signage along the U.S. 101 Freeway and along the project entrances. The landscaping shall effectively screen, to the extent feasible, prominent structures at the pedestrian level, and to the extent possible, help reduce freeway and railroad noises. As currently designed, the Specific Plan provides approximately 22 acres (33%) of green space in the form of parks, streetscape landscaping, corridors, and 101 Freeway/Oxnard Boulevard/Ventura Road frontage landscaping.



Landscaping/Open Space Legend

-  Specific Plan Boundary
-  Roadway Landscape Improvements (Maintained by City of Oxnard through CFD)
-  Common Landscape Areas (Privately Maintained)
-  Community Parks with Amenities (Privately Maintained)
-  Private Recreation Areas (Privately Maintained)
-  Street Landscaping (Maintained by City of Oxnard through CFD)

TOTAL LANDSCAPING/OPEN SPACE = Approximately 22 Acres

5.3 STREETSCAPES

5.3.1 Introduction

The creation of great streets, those that promote the needs of pedestrians over vehicles and provide aesthetically pleasing environments, are an essential element of The Village Specific Plan. This section identifies and describes each of the conceptual streetscape designs proposed for each roadway type and the traffic calming mechanisms proposed as part of the overall roadway network. Figures 5.2 through 5.6 illustrate the overall roadway network and the associated cross-sections for each street design. The plant materials used in these exhibits illustrate the expected scale of plant materials at maturity in order to clearly convey the character envisioned for the roadways.

5.3.2 Main Street

Main Street serves as the primary entrance to the Specific Plan and primary pedestrian and vehicular thoroughfare connecting Oxnard Boulevard with Ventura Road. Main Street will replace the existing disconnected array of roadways serving the existing uses. Two roadway configurations are proposed for Main Street as one moves through the Specific Plan area. For example, Main Street is more urban in character through the Mixed Use Planning Areas (Figure 5.3) and transitions to a more suburban character through the High Density Residential Planning Areas (Figure 5.4). Each configuration is consistent with the

needs of the surrounding uses. The intent behind the design of Main Street is to efficiently convey traffic at reduced speeds and create intimate streetscapes that will encourage pedestrian and bicycle activity.

Within the Mixed Use Planning Areas, Main Street includes one vehicle travel lane in each direction, one Class III bicycle lane on each side of the street, angled parking on each side, and wide sidewalks to promote outdoor dining and pedestrian activity throughout this commercial core.

Within the High Density Residential Planning Areas, Main Street includes a center landscaped median, one vehicle travel lane in each direction, Class II bicycle lanes on both sides, parallel parking on both sides, landscaped parkways, and sidewalks.

Main Street is envisioned as a public street and thus will be maintained by the City of Oxnard.

5.3.3 Neighborhood Streets

The neighborhood streets provide vehicle and pedestrian circulation internal to the blocks. These streets intersect with Main Street at multiple locations for the purposes of providing multiple access points to the residential neighborhoods. The cross-section in Figure 5.5 includes one vehicle travel lane in each direction, parallel parking on both sides, Class III bicycle lanes on both sides, landscaped parkways, and sidewalks. The goal of the neighborhood

CHAPTER 5.0

Landscape Development Plan

street design is to efficiently convey traffic at even lower speeds while continuing to provide a pleasant pedestrian environment.

The neighborhood streets are envisioned as private streets and thus will be privately maintained by the Homeowner's Association.

5.3.4 Alley Street

The alley streets are designed to provide access to the rear-loaded garages within the High Density Residential Planning Areas. They are designed with travel lanes of sufficient width to ensure adequate emergency vehicle access, trash pick-up, and utility maintenance and/or repair. To improve the overall aesthetic, the alleys include a 5-foot wide landscape buffer on each side. For safety reasons, no parking is allowed along any of the alleys (with the exception of temporary parking for service or fire vehicles). Alleys proposed to serve the High Density Residential Planning Areas shall be designed to include appropriate turn-around spaces at the terminus of dead-end alleys, or shall be designed in a loop system intersecting with a neighborhood streets when dwellings front onto a landscaped courtyard, or shall be designed as a service alley intersecting with neighborhood streets when dwellings front onto the street.

Figure 5.6 provides detailed illustrations of the proposed Alley Streets and examples of alley turn-arounds, looped alleys, and service alleys.

The alley streets are envisioned as private streets and thus will be privately maintained by the Homeowner's Association.

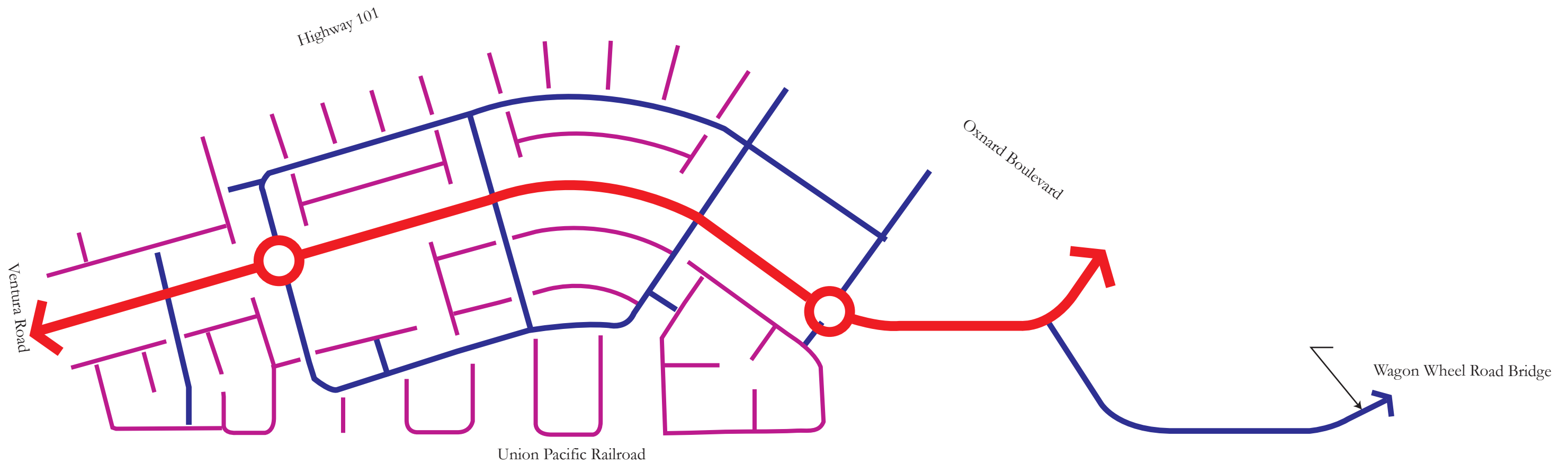
5.3.5 Traffic Calming Mechanisms

Traffic calming mechanisms are integrated into the Specific Plan's overall circulation design. These features also double as unique opportunities for additional landscaping which help to soften the streetscape, and differentiate individual land uses, neighborhoods, and the public realm.

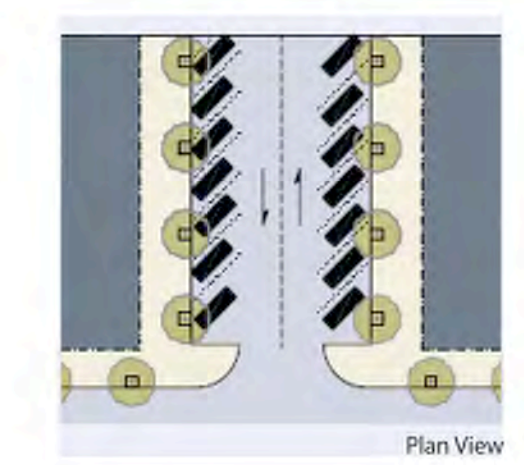
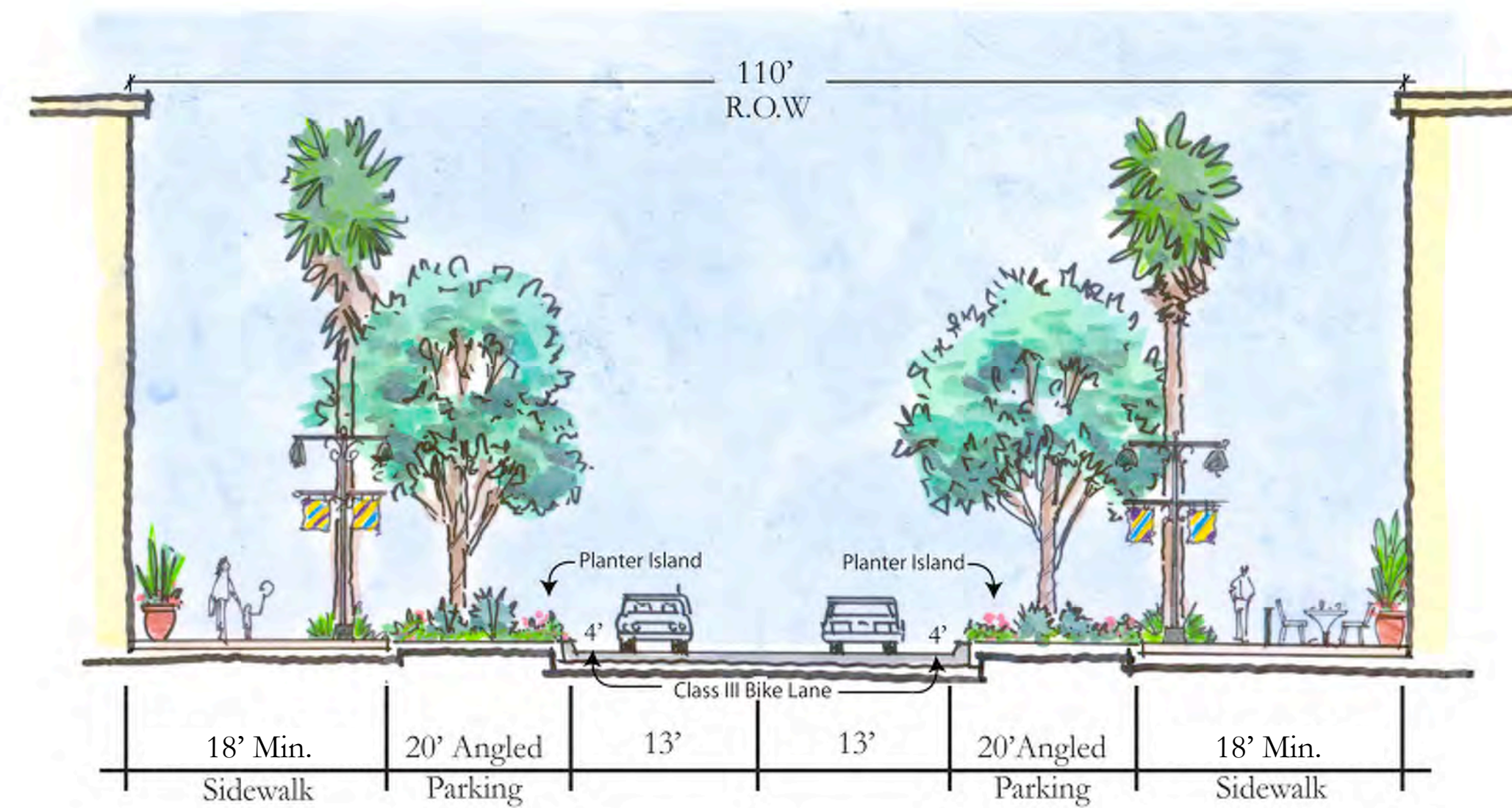
The preferred traffic calming mechanisms are traffic circles with compatible landscaping and signage in their centers; bulb-outs with enhanced intersection and mid-block paving to delineate pedestrian cross-walks, reduce the time needed to cross the street, and tree-lined parkways adjacent to the streets to further define the pedestrian space. Figure 5.7 provides detailed illustrations of the proposed traffic calming mechanisms.

LEGEND

- Main Street (108' & 92' ROW)
- Neighborhood Streets (58' ROW)
- Alley Streets (30' & 36' ROW)



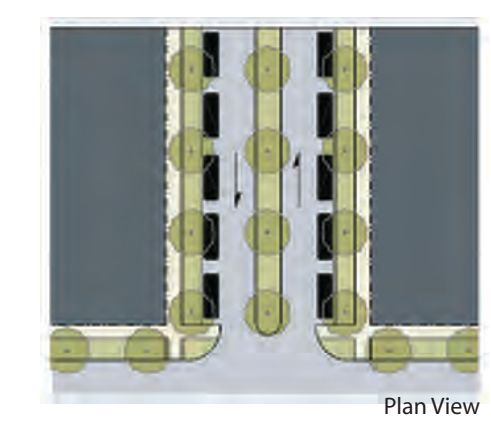
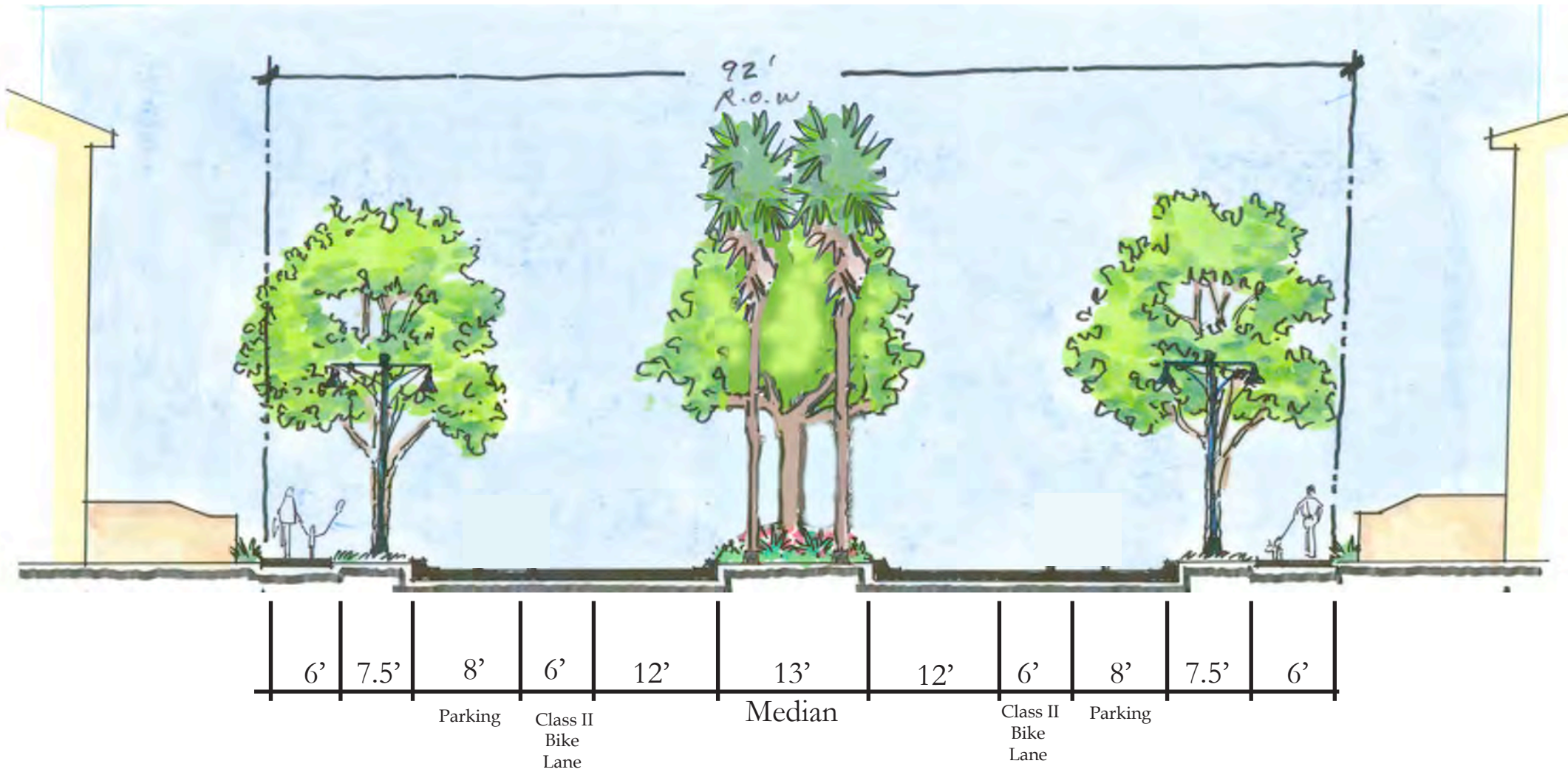
Roadway Network



Thoroughfare Type	Commercial Street
Movement	Slow Movement
Design Speed	25 MPH
Pedestrian Crossing Time	Approx. 6 seconds
Traffic Lanes	2 Lanes
Parking Lanes	Both Sides Angled, Marked
Public Frontage Type	Storefront

Credit: Duany Plater-Zyberk & Co.

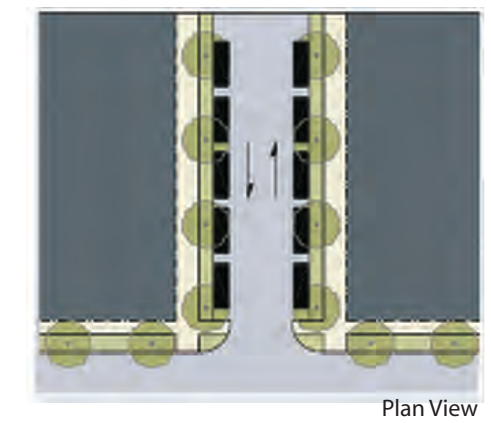
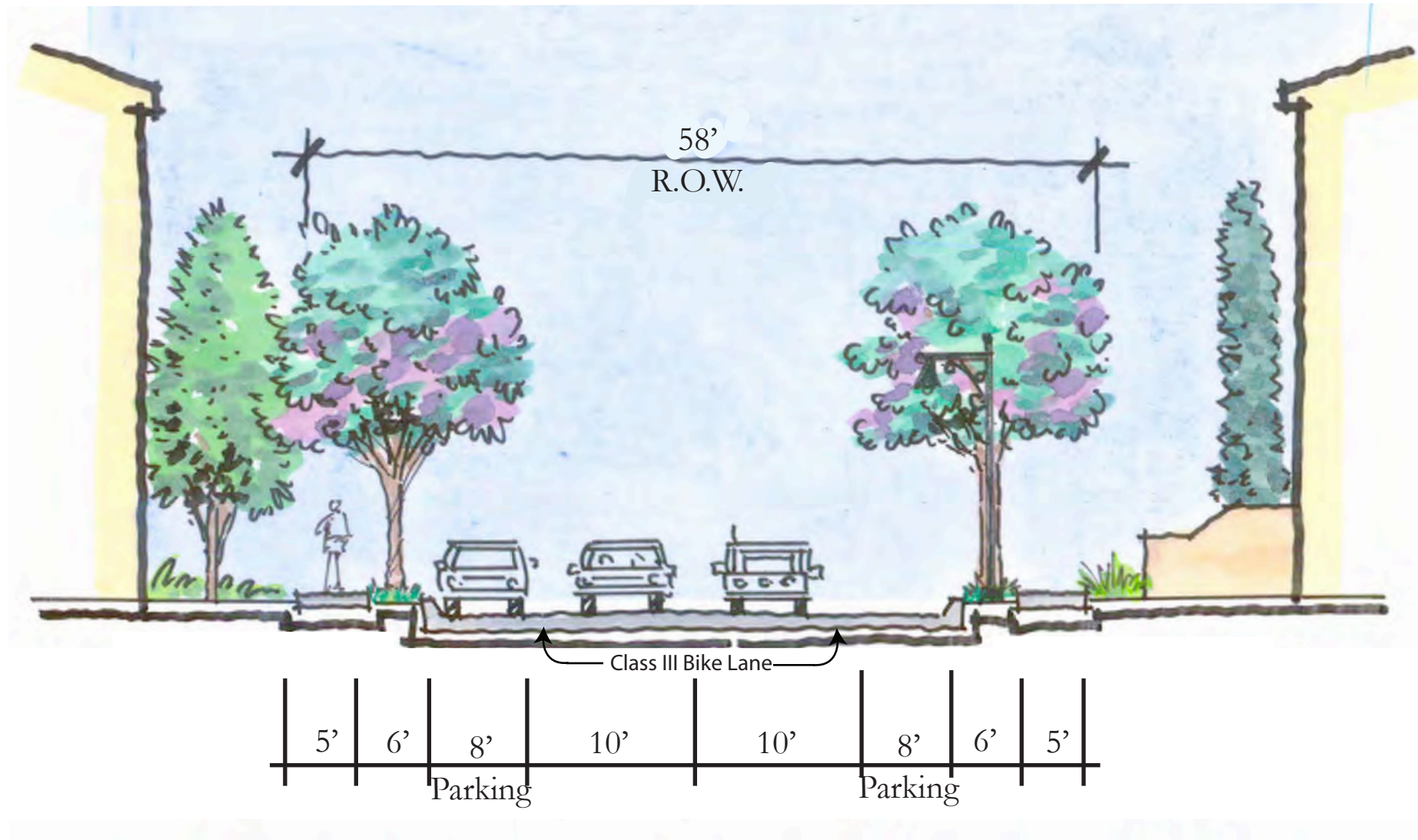
Main Street Commercial Cross Section



Thoroughfare Type	Avenue - Residential
Movement	Slow Movement
Design Speed	25 MPH
Pedestrian Crossing Time	Approx. 13 seconds
Traffic Lanes	2 Lanes
Parking Lanes	Both Sides Parallel - Marked
Public Frontage Type	Townhouse/Rowhouse

Credit: Duany Plater-Zyberk & Co.

Main Street Residential Cross Section



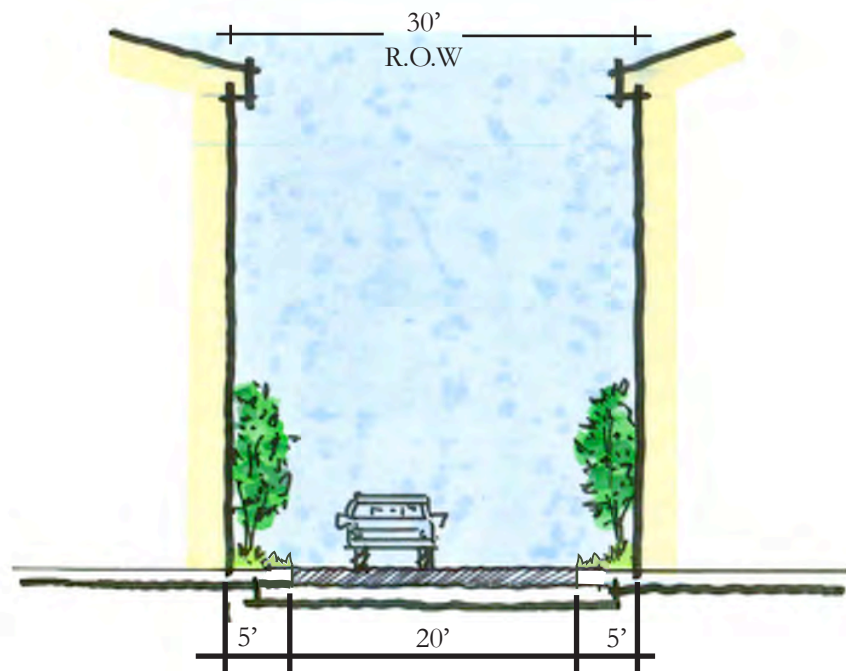
Thoroughfare Type	Neighborhood Residential
Movement	Slow Movement
Design Speed	20 MPH
Pedestrian Crossing Time	Approx. 5.5 seconds
Traffic Lanes	2 Lanes
Parking Lanes	Both Sides Parallel - Marked
Public Frontage Type	Townhouse/Rowhouse/High Rise

Credit: Duany Plater-Zyberk & Co.

Neighborhood Street Cross Section

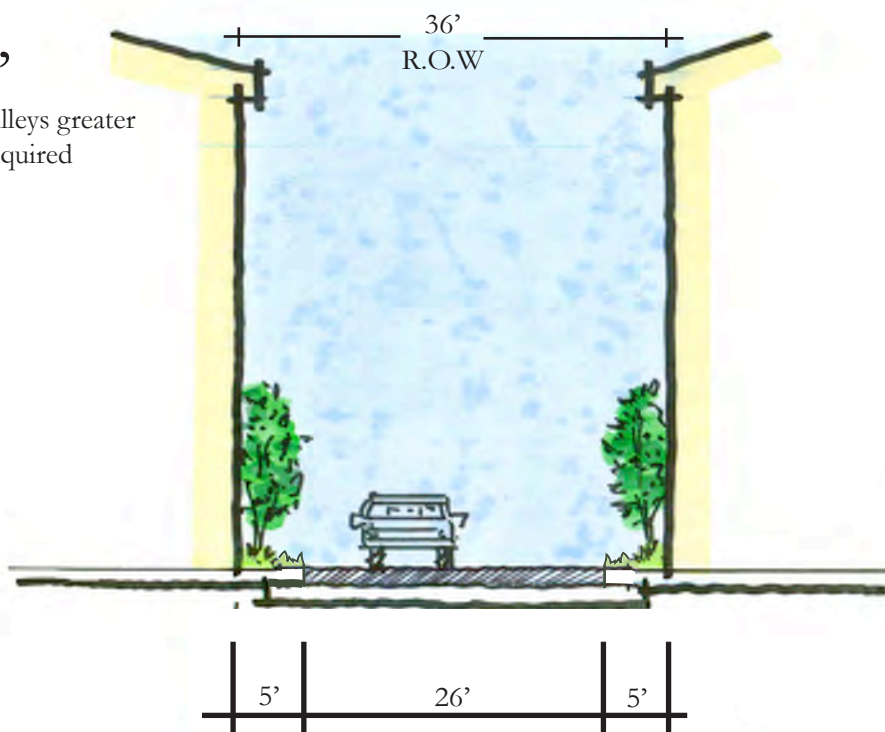
Alley Cross-Section "A"

Note: Alley for Units with Street Frontage.



Alley Cross-Sections "B"

Note: Alley for Paseo/Courtyard Oriented Units or Alleys greater than 150' in length -- Full Fire Department Access Required



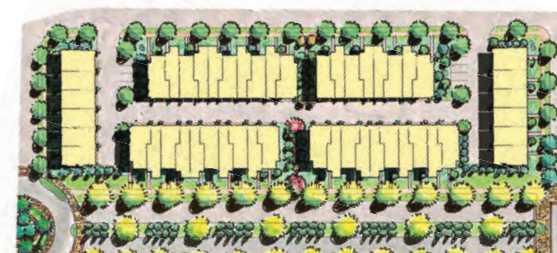
Alley Configurations



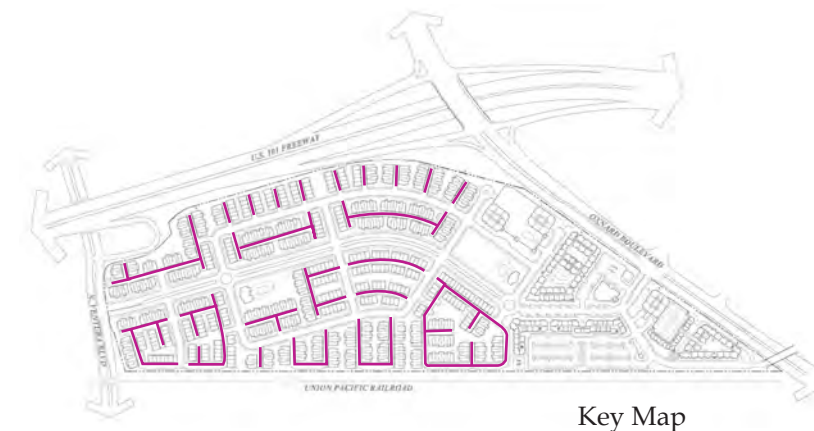
Loop Alley



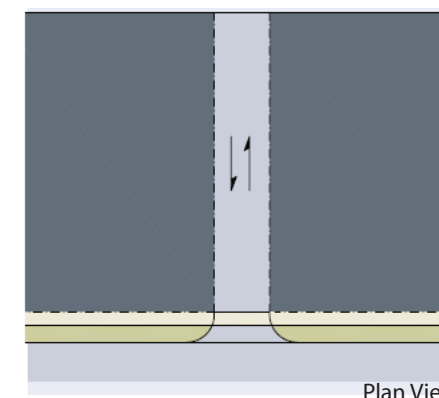
Alley Turn-arounds



Service Alley Connection



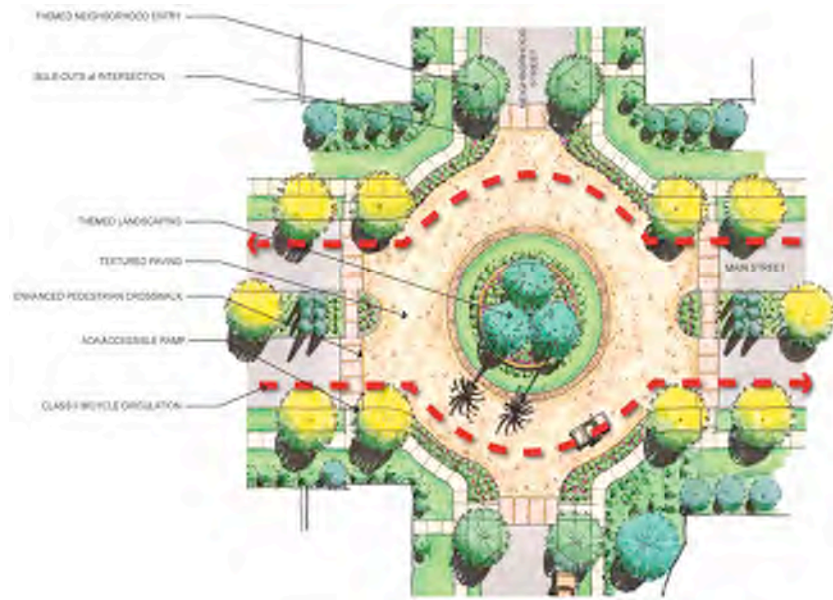
Key Map



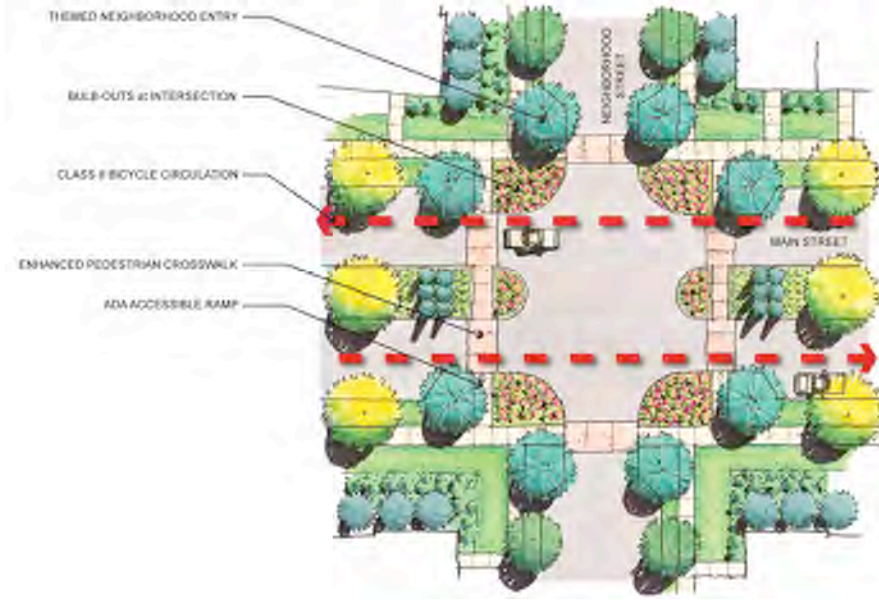
Plan View

Thoroughfare Type	Rear Alley
Movement	Slow Movement
Design Speed	10 MPH
Pedestrian Crossing Time	Approx. 6.5 seconds
Traffic Lanes	2 Lanes
Parking Lanes	None
Public Frontage Type	None

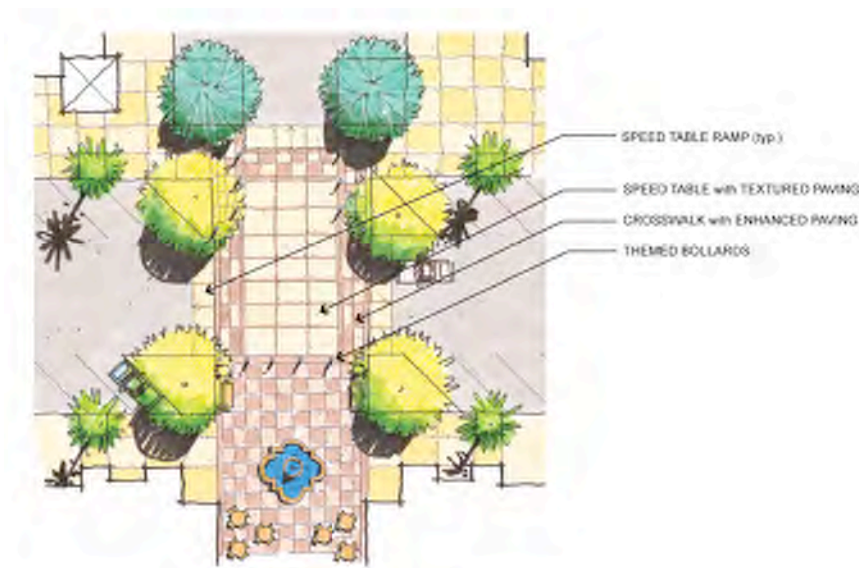
Credit: Duany Plater-Zyberk & Co.



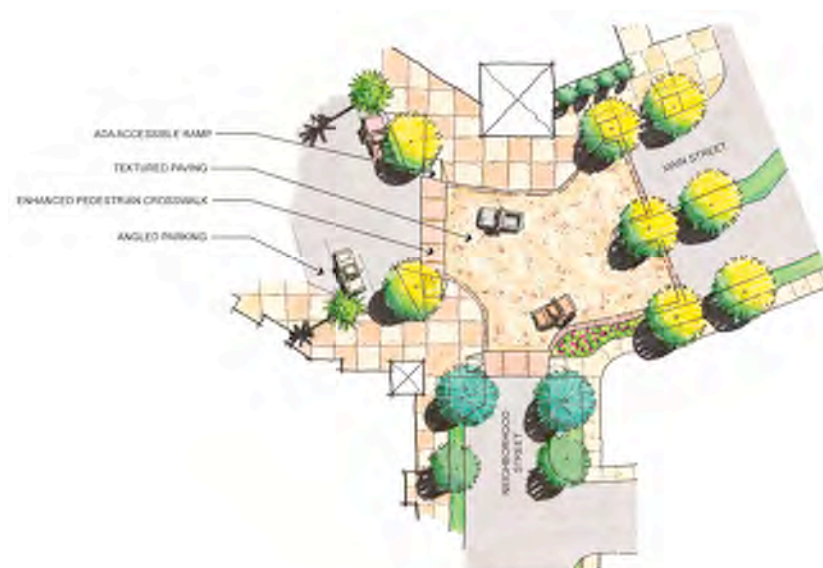
1 Traffic Circle Detail



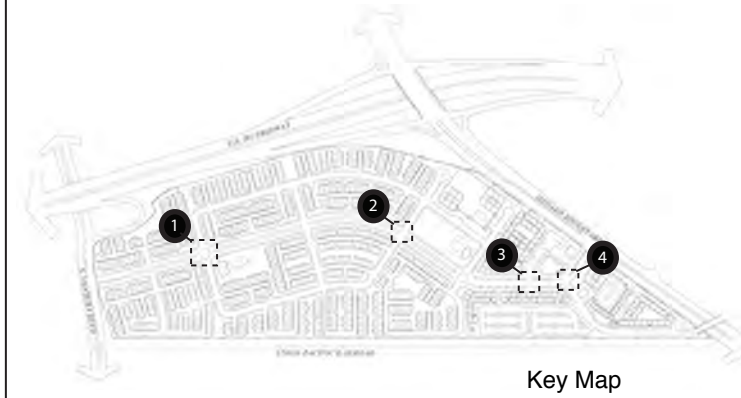
2 4-Way Intersection on Main Street



3 Mid-Block Crossing Detail



4 Main Street/Street "A" Intersection Detail



CHAPTER 5.0

Landscape Development Plan

5.4 DESIGNS CONCEPTUAL PARK

5.4.1 Introduction

With the exception of trails and sidewalks around the perimeter of the Specific Plan area, the plazas, squares and sidewalks along Main Street, and potentially a portion of The Village Green, recreational amenities within the Specific Plan are planned as privately maintained facilities for residents of The Village. As designed, the Village Green (Planning Area 15) and Neighborhood Park (Planning Area 6) provide the focal point for community recreation and include green-space. The greenspace planned for the Village Green could be designated as a public park given its proximity to the public spaces planned within Mixed Use Planning Areas. A determination of public vs. private shall be made prior to recordation of the final map. Several privately maintained pocket parks, courtyards, and pathways are interspersed throughout The Village Specific Plan to provide more passive recreational opportunities for Village residents. Privately maintained terrace and courtyard recreational facilities are also proposed to serve residents of the High Rise, Very High Density Residential, and Mixed Use Planning Areas. These recreation facilities are designed as outdoor rooms and are planned to include formal sensory gardens, enhanced paving, fountains pools, and barbeque areas.

5.4.2 Design Concepts for Each Park

Figures 5.8 through 5.12 illustrate the conceptual designs for parks and recreation areas. Each park design is mintended to be privately maintained by the Homeowner’s Association, except as noted above.



CHAPTER 5.0

Landscape Development Plan

The Village Green is envisioned as one of the central gathering places for residents and visitors of The Village. The turf area is designed to serve multiple functions. A central square is surrounded by lush landscaping, enhanced paving, and sculpted turf which provide opportunities for relaxation, outdoor gatherings, public art, and seasonal festivities such as ice skating, Christmas tree lighting, evening movies in the summer, and outdoor concerts.



CHAPTER 5.0

Landscape Development Plan

The Neighborhood park is also envisioned as one of the central gathering places for residents of The Village and the greater Oxnard community.

This park is planned to include a court play area surrounded by lush landscaping, enhanced paving, and sculpted turf which will provide opportunities for relaxation, active play, and outdoor gatherings.

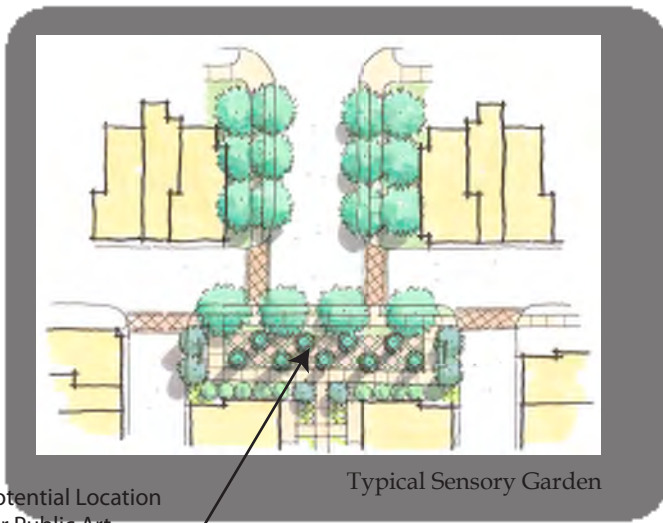


Landscape Development Plan

The pocket parks are envisioned as a series of smaller specialized parks within a five (5) minute walk of the adjacent neighborhood. These parks are designed with intimate landscapes to provide a range of passive and active recreational spaces, such as relaxation, recreation and casual conversation. These pocket parks include seating areas, enhanced paving, water features, formal and informal gardens, and sculpted turf.



Illustrative Photo



Potential Location for Public Art

Typical Sensory Garden

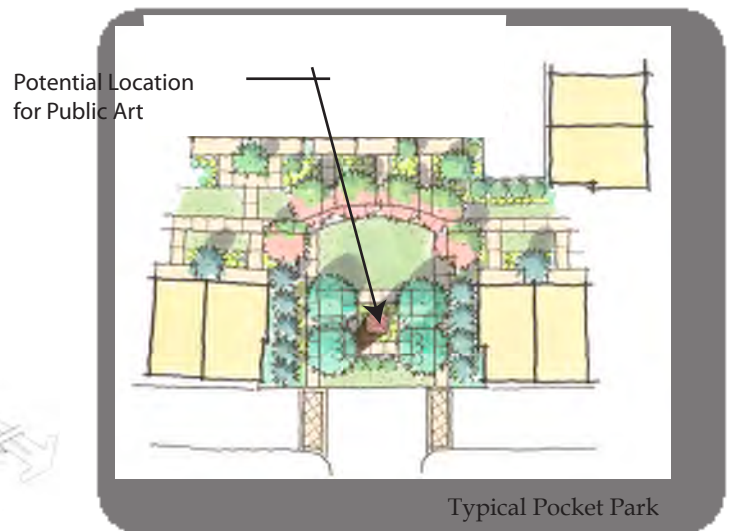


Potential Location for Public Art

Typical Pocket Park



Key Map



Potential Location for Public Art

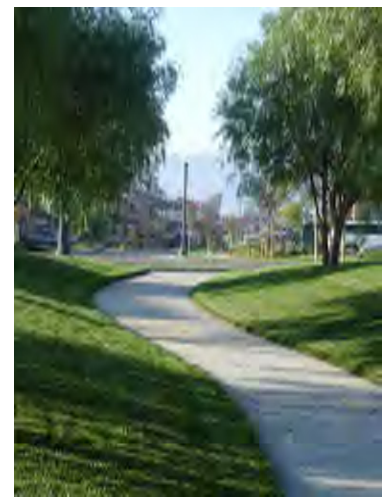
Typical Pocket Park

Pocket Parks

CHAPTER 5.0

Landscape Development Plan

The pedestrian plazas, pathways, and paseos are envisioned as a network of intimate walkway, courtyards and plazas that connect blocks, streets and neighborhoods to one another. They are similar to the landscaped sidewalks with the primary difference being that they provide connections within the block. Paseos or pathways within The Village are present where the block is notably long or deep or where direct access to recreational amenities is desired. These areas are open to the sky or covered for a limited stretch and include landscaping complementary to the adjacent architecture, ample pedestrian amenities (i.e. benches, chairs, and seatwalls) and are illuminated at night by pedestrian scale lighting.



Paseos, Plazas, & Pathways

CHAPTER 5.0

Landscape Development Plan

The High Rise, Mixed Use, and Very High Density neighborhoods within Specific Plan have recreational facilities in either a terrace or courtyard configuration. These facilities are on top of a parking structure or enclosed by a private courtyard. Overall, the facilities will create a resort-style atmosphere, with formal landscaping, enhanced paving, formal gardens, outdoor dining areas, and view terraces.



Illustrative Photo

Concept Pool Terrace Design



Concept Courtyard Design



Courtyard and Terrace Recreational Facilities

5.4.2.1 The Village Green

As depicted in Figure 5.8, the Village Green is designed as one of the primary gathering places for residents and visitors of the Specific Plan. The Village Green and its facilities occupy approximately 1.7 acres. The pool house facility grounds are for the private use of the Village Specific Plan residents and will be maintained by the Homeowner's Association. The green-space adjacent to the west of the pool house has the potential to be a public recreational amenity for the enjoyment of the residents and the greater Oxnard community.

5.4.2.2 The Neighborhood Park

As depicted in Figure 5.9, the Neighborhood Park is designed as one of the primary gathering places for residents of the High Density Planning Areas. The park includes a designated active play area, pool house and pool, and turf areas. The Neighborhood Park is approximately 2.0 acres. These facilities are proposed as a public recreational amenity for the enjoyment of the residents and the greater Oxnard community.

5.4.2.3 Pocket Parks

As depicted in Figure 5.10, the pocket parks are designed to provide more private gathering places within a comfortable 5-minute walk of each residential neighborhood. The pocket parks total approximately 2.24 acres. These facilities are for the private use of

the residents and will be maintained by the Homeowner's Association.

5.4.2.4 Plazas, Paseos and Pathways

As depicted in Figure 5.11, the plazas, paseos, and pathways are designed to provide connections between neighborhoods. Within the mixed use areas, the passageways and plazas will be inviting and attractive spaces for congregation, dining, expansion of retail/commercial space (in the Mixed Use Planning Areas), and pleasing connections to Main Street or parking areas. A Class I bike/pedestrian pathway is proposed along the project's northern edge as an additional public recreational amenity. In addition, the northern most portions of the River Edge Trail will be constructed along the project frontage west of Ventura Road as a public recreational amenity. Interior paseos and trails will be privately maintained by the Homeowner's Association.

5.4.2.5 Private Terrace and Courtyard Recreation Areas

As depicted in Figure 5.12, approximately 1.65 acres are dedicated to private recreation terraces. These facilities are integrated into the building design of the High Rise, Very High Density and Mixed Use Planning Areas, and are proposed to serve the residents of these areas. Access to the facilities will be from either an elevator or private interior courtyard. These spaces are for the private use of the residents and will be maintained by the Homeowners' Association.

CHAPTER 5.0

Landscape Development Plan

5.5 CONCEPTUAL EDGE AND ENTRY LANDSCAPE DESIGNS

5.5.1 Introduction

The perimeter of Specific Plan is planned will be enhanced with gateway landscaping. The design intent is to celebrate the entrance into the City of Oxnard and provide effective visual screening. Figures 5.13 through 5.19 illustrate the perimeter and gateway landscaping concepts.

5.5.2 Perimeter and Entry Landscape Design Concepts

Ventura Freeway Edge: This landscape concept proposes a curvilinear twelve (12) foot high sound wall, raised planted berm, and a 12-foot wide Class I shared bicycle/ pedestrian pathway, with landscaping on each side. Planting on both sides of the freeway wall will serve as a buffer between the Ventura freeway and the residential neighborhoods. The landscaping in the Ventura Freeway edge will be installed by the developer and maintained by the City of Oxnard via a Community Facilities District (CFD). As illustrated in Figure 5.13, this landscaped area occupies approximately 2.21 acres and includes a Class I pedestrian/bicycle pathway, and an attractive “urban forest”.

Wagon Wheel Off-ramp: As shown in Figure 5.14, the re-designed Wagon Wheel off-ramp located northwest of the site is planned to include 0.87 acres of enhanced landscaping and monument signage that will identify the entrance

into the City of Oxnard. The landscaping on the Wagon Wheel off-ramp will be installed by the developer and maintained by the City of Oxnard via CFD. It is important to note that while monument signage is planned at this location, its placement within the freeway right-of-way would require review and approval by Cal-Trans. At the present time there is some uncertainty as to the feasibility of receiving the required approvals from Caltrans for a sign at this location. Nevertheless, the developer shall pay the costs associated with design, fabrication, installation and inspection of a Gateway Freeway Pylon sign at the current location or at an alternative location approved by the City of Oxnard Community Development Department.

Union Pacific Railroad: As Shown on Figure 5.15, the southern boundary of the Specific Plan is proposed to include a 14-foot sound wall, and continuous landscaping along both sides of the wall. The landscaping provides visual separation from the adjacent Southbank residential neighborhood and helps to reduce noise generated by the Union Pacific Railroad and Metrolink. The landscaping will be installed by the developer and maintained by the City of Oxnard via CFD.

Oxnard Boulevard Frontage: As shown in Figure 5.16, a landscaped buffer, 12-foot wide Class I pedestrian/bicycle path, with landscaping on each side. A tree-lined parkway and landscape buffer is proposed to run continuously along the Specific Plan’s Oxnard Boulevard

CHAPTER 5.0

Landscape Development Plan

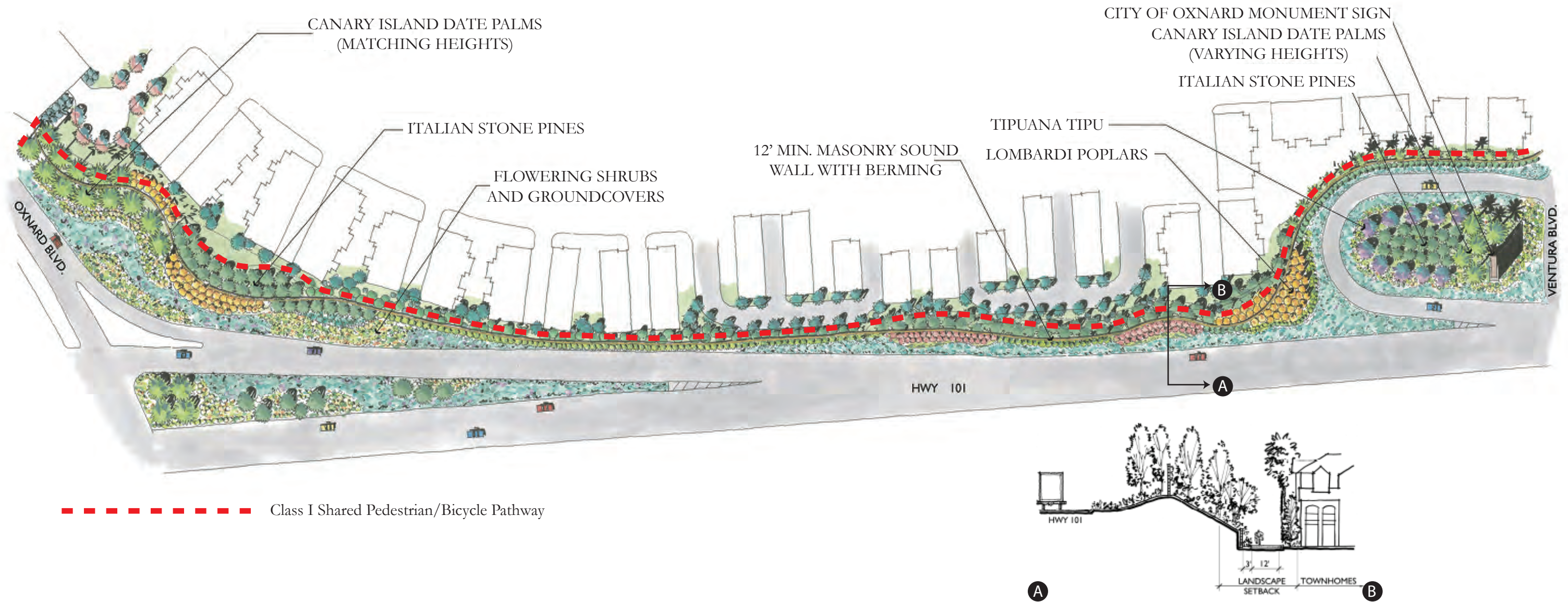
frontage. The landscaped parkway and buffer is planned to include a variety of taller shade trees to provide an attractive green edge and visual screening of buildings and/or parking structures. The landscaping will be installed by the developer and maintained by the City of Oxnard via CFD.

Oxnard Boulevard Entry: The primary entrance to The Village Specific Plan is from the intersection of Oxnard Boulevard and Spur Drive, directly across from the primary entrance to the Esplanade Shopping Center. As shown in Figure 5.17, the entry program will include thematic architectural forms all in keeping with a European style. Specific landscaping elements include decorative paving, decorative entry walls, decorative signage, and formal landscaping. The landscaping will be installed by the developer and maintained by the City of Oxnard via CFD.

Ventura Road Frontage: As shown in Figure 5.18, a landscaped buffer, 12-foot wide Class I shared bicycle/pedestrian pathway with landscaping on each side. A tree-lined parkway and a landscaped buffer are proposed to run continuously along The Village Specific Plan's frontage along Ventura Road between Highway 101 and Union Pacific Railroad (Figure 5.18). The landscaped parkway and buffer include a variety of taller shade trees to provide an attractive green edge and visual screening of buildings. The landscaping will be installed by the developer and

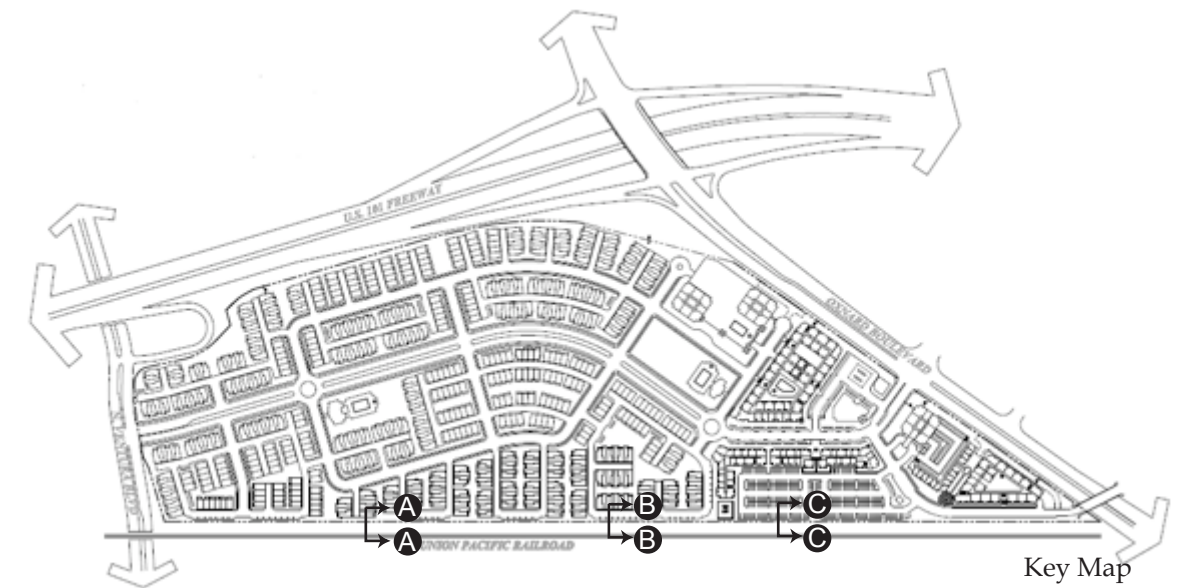
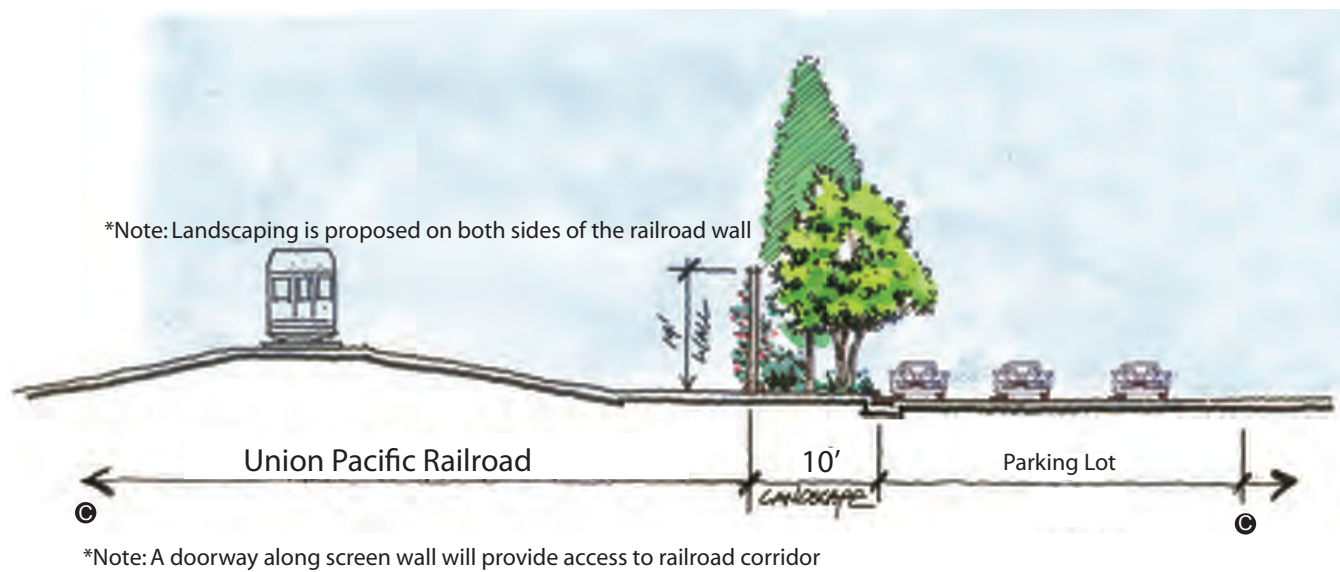
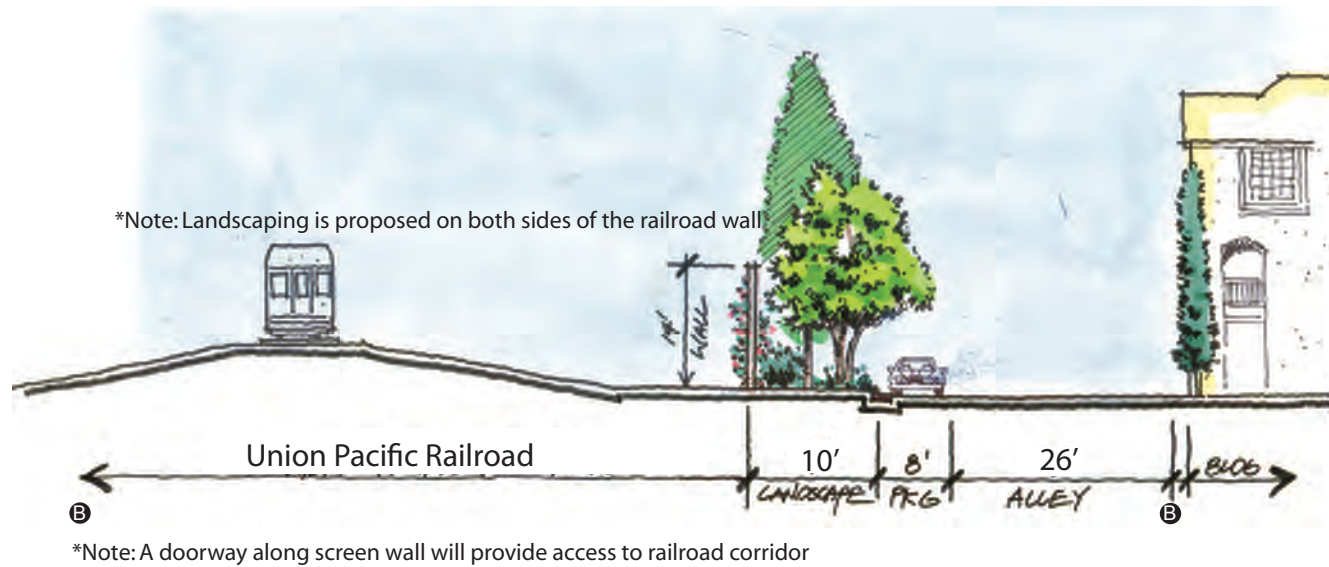
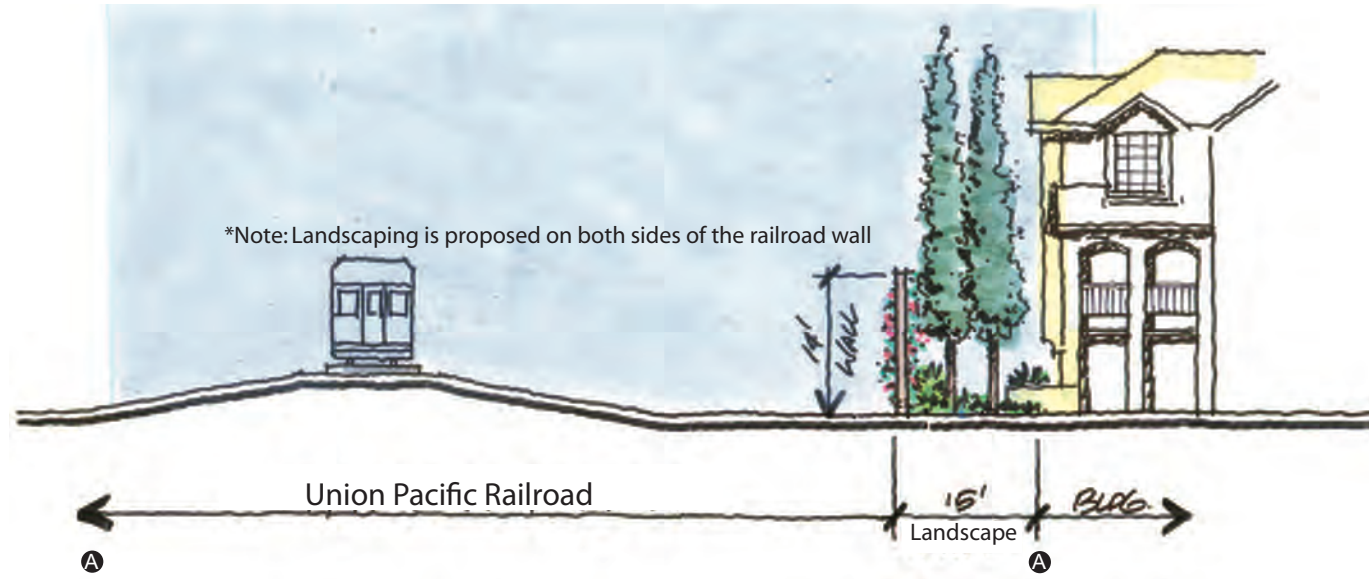
maintained by the City of Oxnard via CFD.

Ventura Road Entry: The secondary entrance into The Village Specific Plan from Ventura Road makes a dramatic landscape statement along the Ventura Road frontage. As shown in Figure 5.19, this entry area includes decorative entry walls, enhanced paving, formal European style landscaping, and decorative signage. The landscaping will be installed by the developer and maintained by the City of Oxnard via CFD.

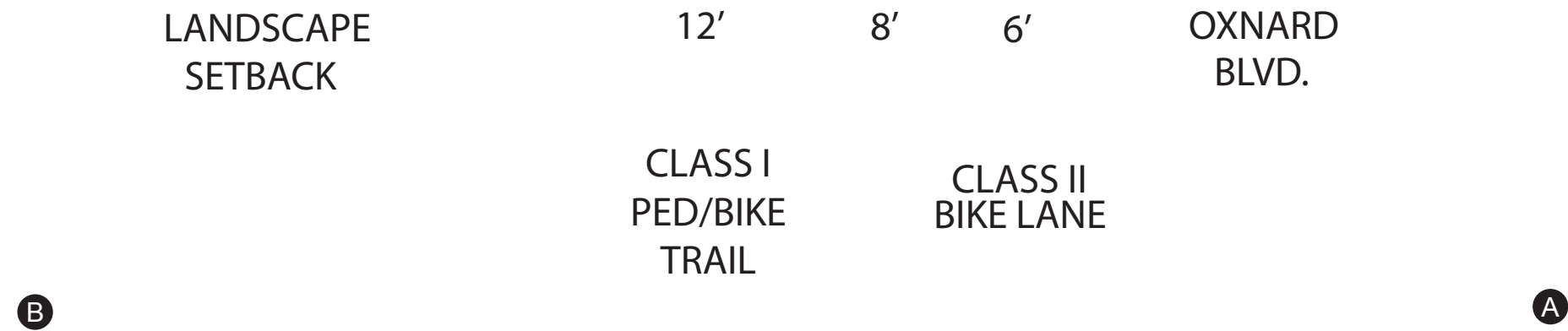
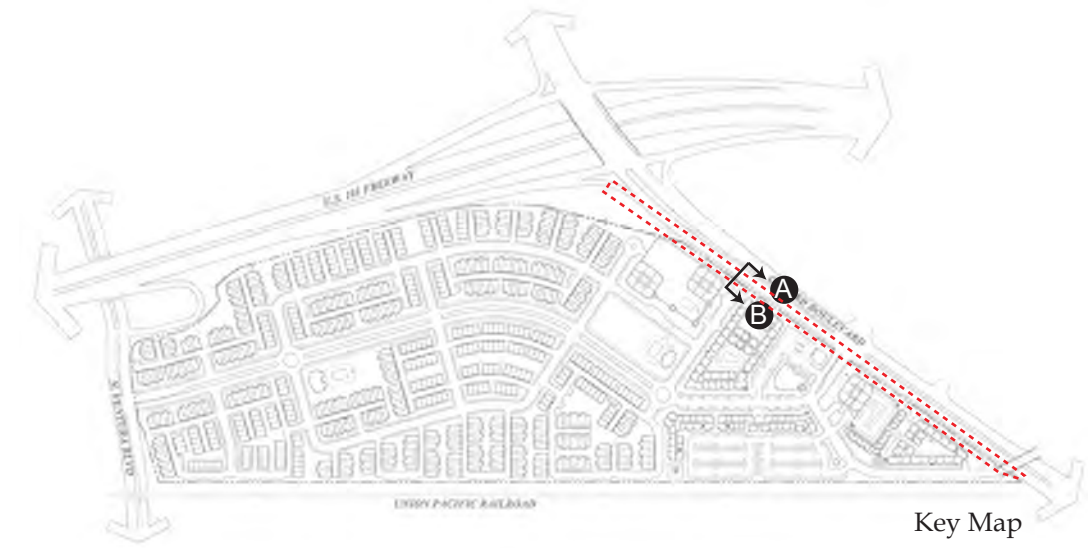




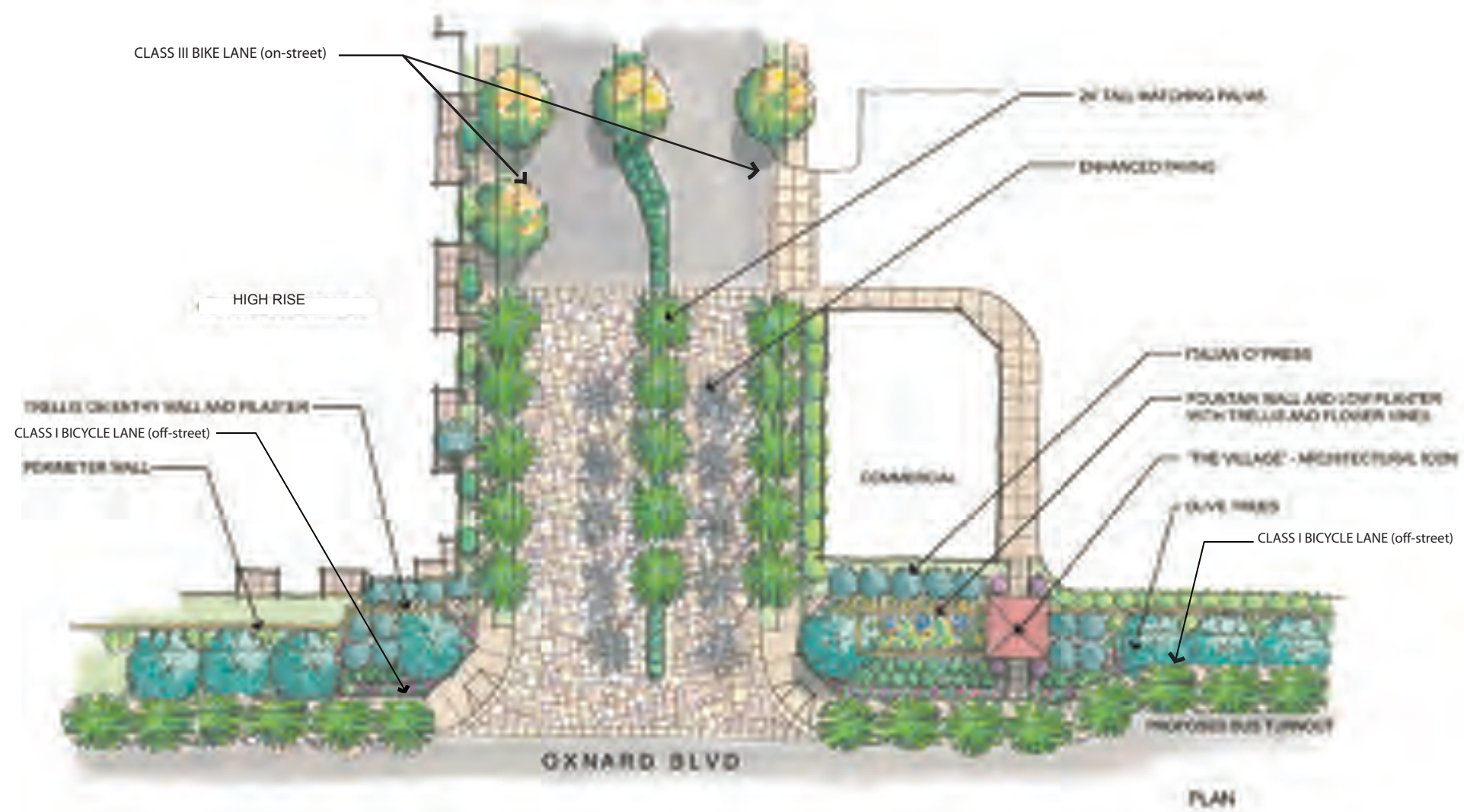
Note: Rendering is for conceptual planning purposes only and is subject to change based upon final location of City gateway signage.



Union Pacific Railroad Edge Cross-Sections



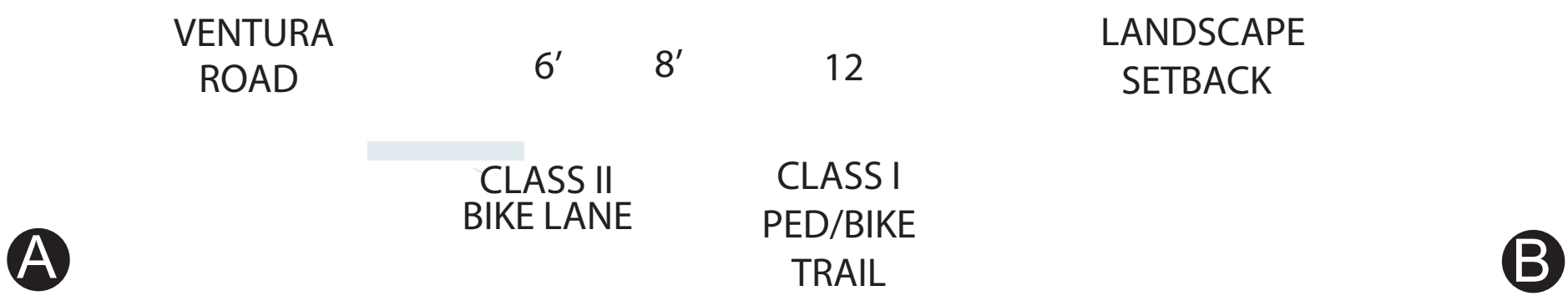
Oxnard Boulevard Cross Section



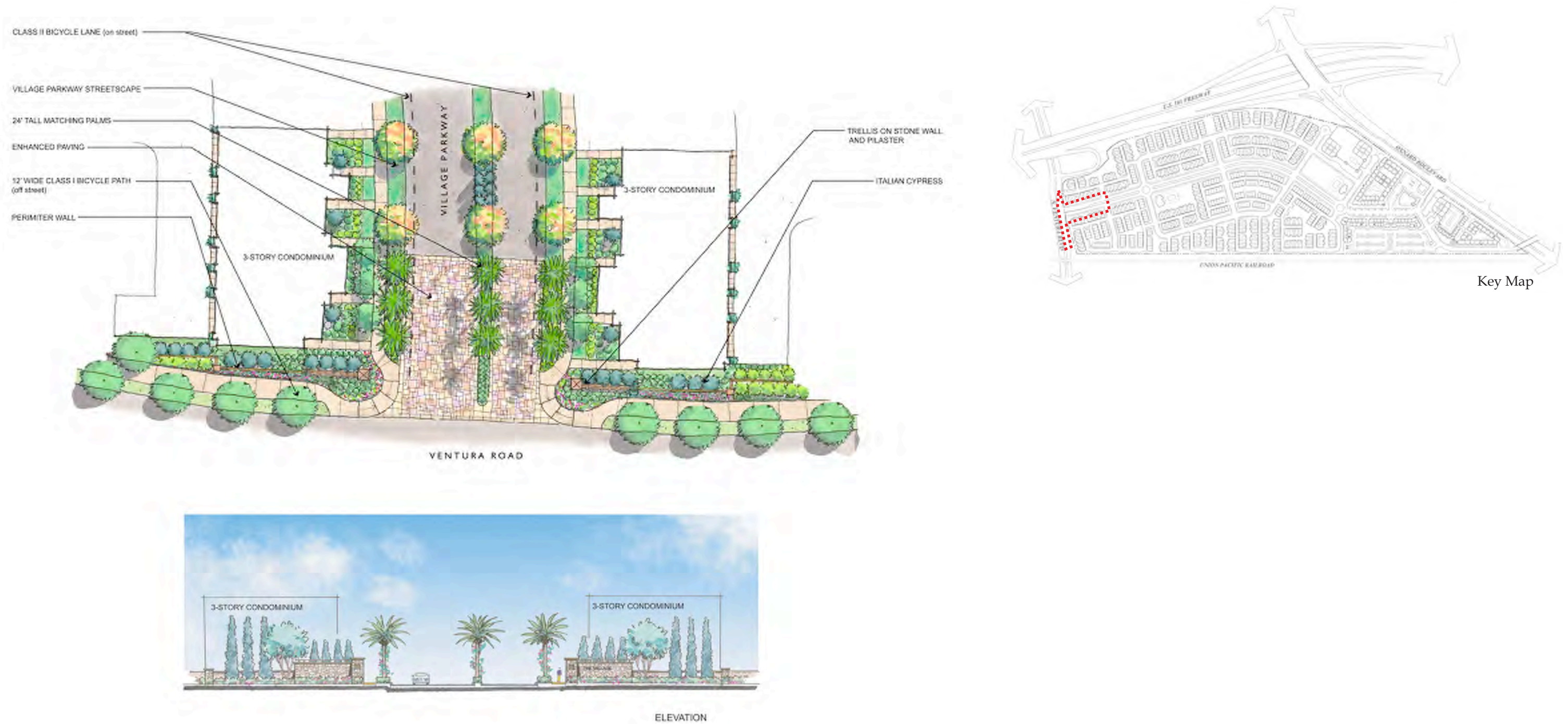
Oxnard Boulevard Entry Landscaping



Key Map



Ventura Road Cross Section



Ventura Road Entry Landscaping

5.6 NATURAL STORM WATER MANAGEMENT

5.6.1 Introduction

Several landscaped areas within the boundaries of The Village Specific Plan will provide the opportunity for use of storm water Best Management Practices (BMPs) to capture and treat storm water before it enters into the Santa Clara River. The Specific Plan's general approach for use of natural landscapes as storm water management systems is described below.

5.6.2 Approach

The landscaped environment within the Specific Plan can be combined with storm water BMPs to effectively reduce pollutant loads associated with site runoff and can potentially reduce runoff volumes during a storm event. Natural treatment systems add to the aesthetic quality of the community and can address several aspects of storm water management. For example, filter strips, bio-swales, and bio-retention gardens can be utilized as part of parkway, parking lot, and building landscaping areas to provide opportunities for groundwater recharge, sediment drop out, and pollutant filtration. Planting mature trees within parkways and parking lots hold rainwater in their leaf canopies and increase porosity in soils thus increasing infiltration and percolation. Landscaped corridors and parks can double as smaller detention basins and/or infiltration basins; and decorative porous paving can be used

within plazas and roadways to provide infiltration and pollutant filtration. Figure 5.20 illustrates some of the opportunities for integrating natural storm water BMPs within the landscaped areas of the Specific Plan.



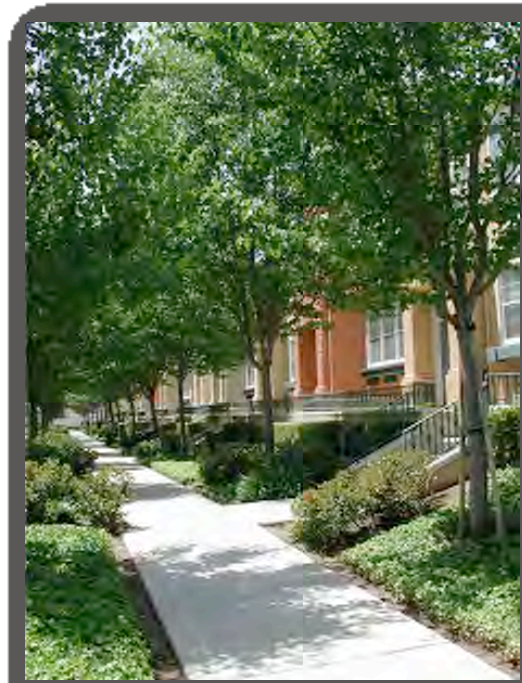
Parks and paseos/corridors can provide stormwater detention/infiltration



Green roofs on buildings and parking structures provide additional runoff collection and treatment areas



Parking lot landscaping can be used as stormwater collection and treatment areas

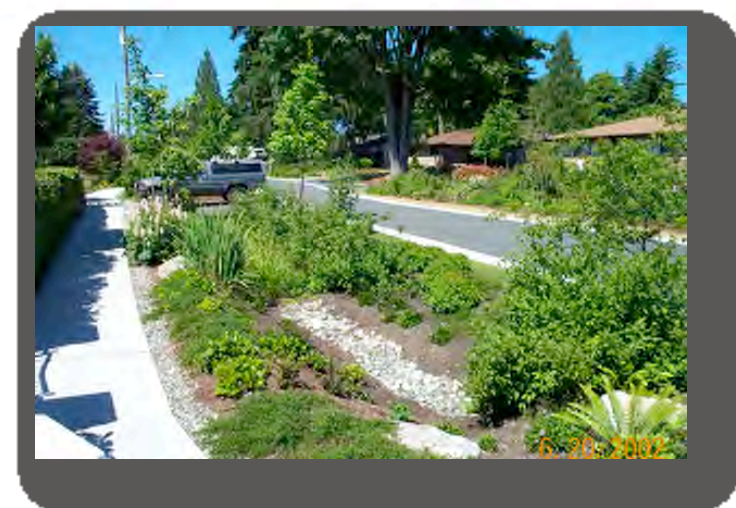


Mature trees capture rainwater and improve soil infiltration capacity



Natural Stormwater BMP Legend

- Parks/Paseos/Corridor Landscaping BMPs
- Parkway/Parking Lot Landscaping BMPs



Landscaped parkways can provide stormwater detention and filtration

Natural Landscaping Stormwater Management Opportunities

5.7 LANDSCAPING PLANT MATERIALS

The objective of the overall landscaping concept is to provide a distinct visual impression of community identity, soften the urban experience, and provide the highest level of aesthetic standards. The landscaping concept is complimented by the quality of the building materials that will assure an attractive environment and enhance the quality of life for its residents.

The list of plant materials as specified on Table 5.1 is intended to serve as a guide during preparation of formal landscaping plans. While exact plant species conformance is not required, the use of plant species not listed in the table below must be proven to have comparable drought tolerances and aesthetic quality, and must not be invasive. Figure 5.21 - Street Tree Plan identifies the desired street tree species, acceptable sizes, and general location.

CHAPTER 5.0

Landscape Development Plan

Table 5.1 - Landscape Plant Materials List

TREES and PALMS

Botanical Name	Common Name
MAIN STREET (Publicly Maintained)	
<i>Street Trees (minimum 36" box size)</i>	
Cinnamomum camphora	Camphor Tree
<i>Background / Spatial Definition Tree (minimum 24" box size)</i>	
Ficus 'florida'	Ficus
Pinus eldarica	Afghan Pine
Prunus caroliniana	Carolina Cherry
Tristania conferta	Brisbane Box
Washingtonia robusta (min. 10' brown trunk)	Mexican Fan Palm (<i>Commercial Zone</i>)
MAIN STREET MEDIANS (Publically Maintained) (minimum 36" box size)	
Tipuana Tipu Palm Species	Tipu Tree
ROUND-ABOUTS	
<i>Dominant Trees (minimum 24" box size)</i>	
Olea europaea	Olive
Phoenix canariensis (min. 12' brown trunk)	Canary Island Palm (<i>matching Hts.</i>)
NEIGHBORHOOD STREETS (Privately Maintained)	
<i>Street Trees (minimum 36" box size)</i>	
Platanus acerifolius	London Plane Tree
Tipuanu tipu	Tipu Tree
Ulmus parviflora 'Drake'	Evergreen Elm

CHAPTER 5.0

Landscape Development Plan

Botanical Name	Common Name
----------------	-------------

Background / Spatial Definition Trees (minimum 24" box size)

Magnolia g. 'Little Gem'	Southern Magnolia
Pinus canariensis	Canary Island Pine
Tristania conferta	Brisbane Box
Botanical Name	Common Name

ALLEYS (Privately Maintained)

Dominant Trees (minimum 24" box size)

Prunus c. Bright 'n Tight	Carolina Cherry
Raphiolepis 'Majestic Beauty'	India Hawthorn (<i>Tree Form</i>)

OXNARD BOULEVARD (Publicly Maintained)

Dominant Tree (City Street Tree) (minimum 36" box size)

Cupaniopsis anacardioides	Carrotwood
---------------------------	------------

Background: (minimum 24" box size)

Phoenix canariensis (min. 12' brown trunk)	Canary Island Palm
Pinus eldarica	Afghan Pine
Populus italica	Lombardy Poplar
Prunus caroliniana	Carolina Cherry
Tristania conferta	Brisbane Box
Tipuana Tipu	Tipu

101 FREEWAY LANDSCAPING (Publicly Maintained)

Dominant Trees (minimum 24" box size)

Tipuana Tipu	Tipu
Phoenix canariensis (min. 12' brown trunk)	Canary Island Palm
Pinus eldarica	Afghan Pine
Populus italica	Lombardy Poplar
Prunus caroliniana	Carolina Cherry
Sequoia sempervirens	Coast Redwood
Tristania conferta	Brisbane Box

PARKS AND COMMON OPEN SPACE (Privately Maintained)

Dominant Trees (minimum 24" box size)

Olea europaea	Olive
Pinus pinea	Italian Stone Pine
Pyrus kawakamii	Evergreen Pear
Washingtonia robusta (min. 10' brown trunk)	Mexican Fan Palm

CHAPTER 5.0

Landscape Development Plan

Botanical Name	Common Name
----------------	-------------

Accent Trees: (minimum 24" box size)

Jacaranda mimosifolia	Jacaranda (<i>Used sparingly</i>)
Lagerstroemia indica	Crape Myrtle
Pyrus calleryana "Aristocrat"	Bradford Pear

Background: (minimum 24" box size)

Cupaniopsis anacardioides	Carrtoewood
Pinus eldarica	Afghan Pine
Prunus caroliniana	Carolina Cherry
Prunus c. Vesuvius	Purple-leaf Plum
Tristania conferta	Brisbane Box
Tipuanu tipu	Tipu Tree
Washingtonia robusta (min. 10' brown trunk)	Mexican Fan Palm

TERRACES/COURTYARDS (Privately Maintained)

Dominant Trees (minimum 24" box size)

Archontophoenix cunninghamiana (min. 12' brown trunk)	King Palm
Olea europaea	Olive
Washingtonia robusta (min. 12' brown trunk)	Mexican Fan Palm

Accent Trees (minimum 24" box size)

Erythrina crista-galli	Coral Tree
Magnolia 'Little Gem'	Southern Magnolia
Melaleuca nesophila	Pink Melaleuca
Pyrus kawakamii	Evergreen Pear
Tipuanu tipu	Tipu Tree

PERIMETER LOT LINES (Privately Maintained)

Dominant Trees (minimum 24" box size)

Eucalyptus citriodora (max. 15 gallon)	Lemon-scented Eucalyptus
Pinus eldarica	Afghan Pine
Populus italica	Lombardy Poplar
Sequoia sempervirens	Coast Redwood
Tristania conferta	Brisbane Box

CHAPTER 5.0

Landscape Development Plan

SHRUBS and GROWDCOVERS

Botanical Name	Common Name
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FRONT YARD STREET LANDSCAPE SETBACKS (Privately Maintained)

Sodded Turf Parkway

Water-conserving tall fescue

Large shrubs (5 gallon minimum size at 3' o.c.)

Trachelospermum jasminoides	Star Jasmine	(foreground)
Raphiolepis clara	India Hawthorn	(middleground)
Dietes bicolor	Butterfly Iris	(middleground)
Strelitzia reginae - 15 g.	Bird of Paradise	(background)
Ligustrum texanum - 15 g.	Glossy Privet	(background)

PARKS AND COMMON OPEN SPACE (Privately Maintained)

Large shrubs (5 gallon minimum size at 3' o.c.)

Bambusa oldhamii	Clumping Giant Bamboo
Camellia japonica	Camellia
Feijoa sellowiana	Pineapple Guava
Juniperus torulosa	Hollywood Juniper
Leptospermum laevigatum	Australian Tea Tree
Ligustrum texanum	Glossy Privet

Medium Shrubs (5 gallon minimum size)

Buxus 'green beauty'	Boxwood
Camellia sasanqua	Camellia
Dietes bicolor	Butterfly Iris
Grevillia species	Grevillia
Hemerocallis hybrids	Evergreen Daylilies
Phormium 'maori queen'	Flax
Pittosporum species	Mock Orange
Raphiolepis indica	India Hawthorn
Strelitzia reginae	Bird of Paradise

Low Shrubs and Groundcovers (1 gallon minimum size)

Agapanthus 'Peter Pan'	Dwarf lily of the Nile
Hedera h. 'Needlepoint'	needle point ivy
Lantana montevidensis	lantana
Limonium perezii	sea lavender
Rosmarinus 'prostrata'	prostrate rosemary
Trachelospermum jasminoides	star jasmine

CHAPTER 5.0

Landscape Development Plan

Botanical Name	Common Name
----------------	-------------

Accent/color shrubs (5 gallon minimum size)

Aloe species	Aloe
Agave attenuata	Mexican Agave
Canna species	Dwarf Canna
Pennisetum cupreum	Red Fountain Grass

RAISED PLANTERS ON PODIUM (Privately Maintained)

Large Shrubs (5 gallon minimum size at 3' o.c.)

Bambusa oldhamii	Oldham Bamboo
Howea forsterana	Paradise Palm
Philodendron selloum	Big Leaf Philodendron
Strelitzia nicolai	Giant Bird of Paradise
Pittosporum engenioides	n.c.n.
Pittosporum tobira	Tobira
Schefflera arboricola	Hawaiian Elf Schefflera

Medium Shrubs (5 gallon minimum size at 3' o.c.)

Camelia species	Camelia
Cycas revoluta	Sago Palm
Nandina domestica	Heavenly Bamboo
Nephrolepis exaltata	Sword Fern

Small shrubs (5 gallon minimum size at 3' o.c.)

Aspidistra elatior	Cast Iron Plant
Clivia miniata	Kaffir Lily
Codiaeum variegatum	Croton
Liriope gigantea	Liriope

101 FREEWAY CALTRANS ROW LANDSCAPING (Publicly Maintained)

Large Shrubs (5 gallon minimum size at 3' o.c.)

Acacia redolens	Prostrate Acacis
Heteromeles arbutifolia	Toyon
Lantana montevedensis	Lantana
Leucophyllum frutescens	Texas Ranger
Myoporum parvifolium	Myopourum
Plumbago ariculata	Cape Plumbago
Raphiolepis indica	India Hawthorn

Line of Sight Note: Maintain shrubs at 24" high inside of Line of Sight.

Landscape Development Plan

MAIN STREET (RESIDENTIAL)



Olive Tree



Camphor Tree

ROUNDBABOUT



Olive Tree



Canary Island Palm

OXNARD BOULEVARD/VENTURA ROAD



Carrotwood Tree



NEIGHBORHOOD STREETS



London Plane Tree



Tipu Tree

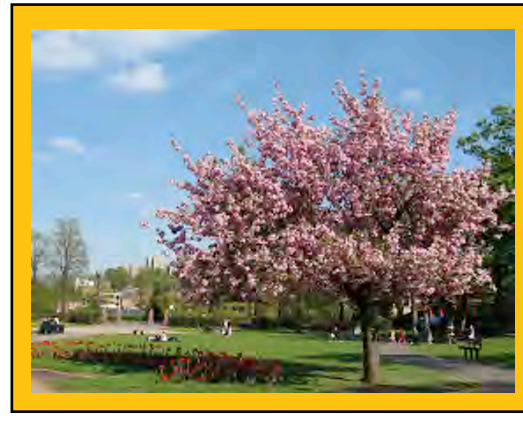


Evergreen Elm

ALLEY TREES



Carolina Cherry



India Hawthorn (Tree Form)

Street Tree Plan



The Village

SPECIFIC PLAN



Chapter 6

Infrastructure Development Plan

CHAPTER 6.0

Infrastructure Development Plan

6.1 INTRODUCTION

This Chapter addresses the transportation, infrastructure, and other engineering components of The Village Specific Plan. Note that the Environmental Impact Report (EIR) accompanying this Specific Plan also addresses infrastructure and may include additional requirements to help ensure that the surrounding infrastructure can support the project. The following topics are discussed in this Chapter of the Specific Plan:

- Demolition and Grading
- Drainage and Water Quality
- Water Infrastructure
- Sewer Infrastructure
- Dry Utility Infrastructure
- Roadway Design
- Parking
- Transportation Demand Management

The infrastructure plans, demolition plans, grading plans, drainage plans, parking and circulation plans described in this Chapter are not precisely designed or “engineered” for construction as part of The Village Specific Plan. The plans presented in this Chapter are conceptual and thus are subject to change as the detailed engineering designs are prepared, reviewed, and approved by the City of Oxnard. However, the plans presented in this Chapter do describe the extent of the proposed infrastructure improvements, the conceptual engineering designs intended to establish the standards by which the final precise infrastructure plans must conform, and

programs which address transportation demand management and parking.

6.2 DEMOLITION AND GRADING

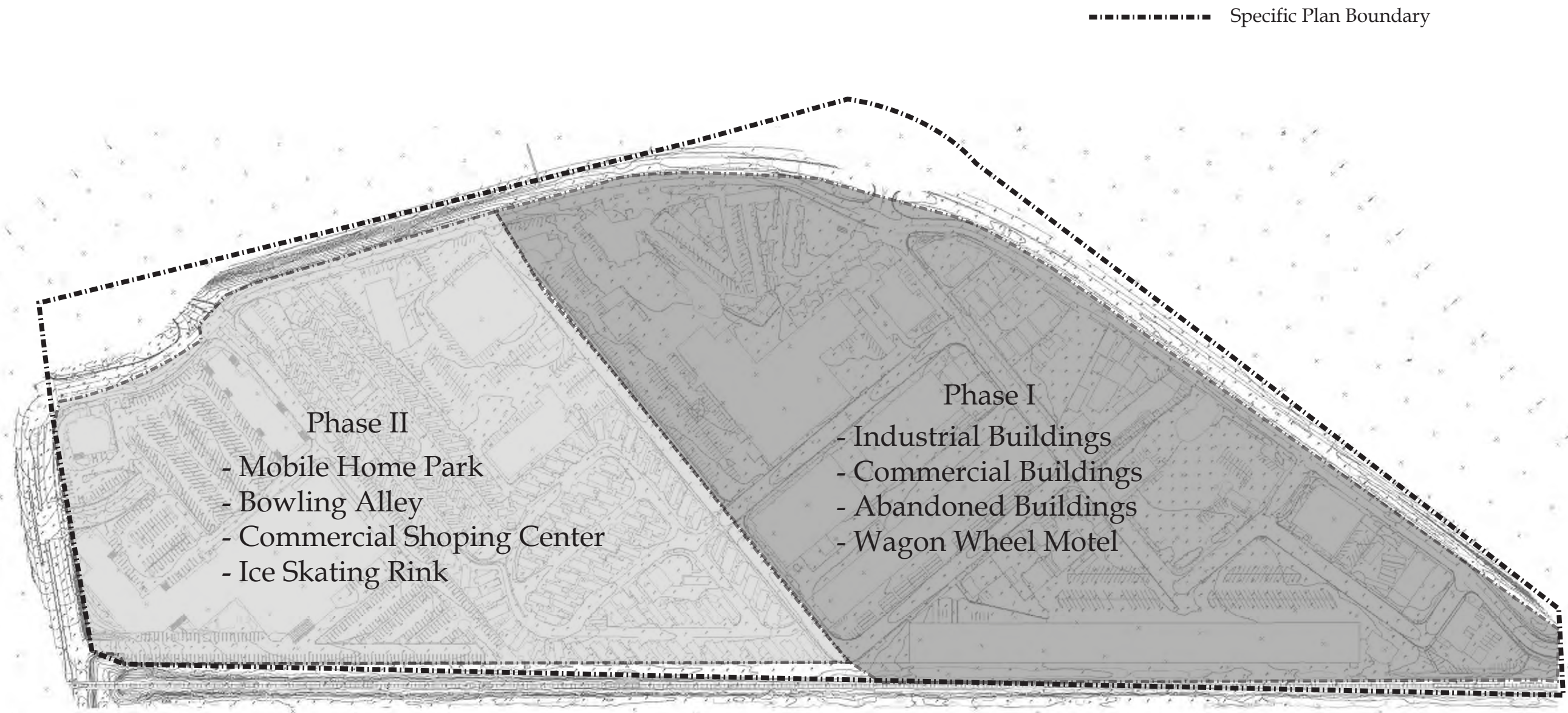
6.2.1 Existing Conditions

The existing topography of the site is relatively flat, and the site is fully improved with wet and dry utilities, roadways, commercial and industrial buildings, and a mobile home park. In addition, there are a number of mature trees and shrubs on-site which were planted as part of the site’s formal and informal landscaping.

6.2.2 Proposed Conditions

The proposed demolition plan for The Village involves the complete removal of all existing structures, roadways, and existing landscaping within the boundaries of the Specific Plan. Demolition is planned to be completed in two phases as illustrated on Figure 6.1 and outlined in Section 8.5. During demolition, building materials will be recycled and re-used as part of new construction, and the existing mature trees will be preserved and re-used to the extent feasible.

The proposed grading plan maintains the existing topographic condition, with slight changes (+/- one-foot) in overall elevations to establish adequate drainage, street grades, subterranean garage construction, and building pad envelopes. The pad elevations also take into account the need for project phasing.



CHAPTER 6.0

Infrastructure Development Plan

6.3 DRAINAGE AND WATER QUALITY

6.3.1 Existing Conditions

Existing development within the boundaries of the Specific Plan area has established impervious surfaces over approximately 97% of the site. As a result, the majority of storm water runoff currently sheet flows towards the southern and western portions of the site into existing drain inlets that convey flows into the El Rio Drain. A small portion of the site drains to the east towards Oxnard Boulevard. With the exception of three existing storm drain inlets on-site, no other formal drainage infrastructure has been constructed. The existing peak runoff volume from a 100 year storm is estimated to be 185 cubic feet per second (cfs). All of the site runoff currently drains directly into the El-Rio Drain and ultimately into the Santa Clara River.

6.3.2 Proposed Conditions

Development of The Village Specific Plan will require the construction of new drainage infrastructure to reduce existing drainage and flooding problems present within areas immediately off-site. In addition to the construction of conventional drainage improvements like catch basins and storm drains, the Specific Plan envisions using sustainable drainage technologies, such as subterranean mechanical water quality filtration, natural storm water infiltration areas, mechanical storm water filtration components, and if necessary, under-

ground detention basins. Overall, the drainage facilities are designed to reduce flooding caused by the El-Rio Drain (which is currently over capacity) and the Santa Clara River during heavy rains, and reduce the runoff volume and the concentration of contaminants within the storm water from the site before it enters the Santa Clara River. Drainage studies completed as part of the Specific Plan show that development of the subject site alone without any significant drainage improvements will result in a reduction of off-site drainage volumes by approximately 15 percent. The studies also show that the drainage improvements will remove the existing drainage volumes currently conveyed into the El Rio Drain and outlet them directly into the Santa Clara River at a reduced volume. Conformance with the storm water management objectives below will also help ensure that development cleanses storm water prior to leaving the site, thus reducing storm water pollutant concentrations.

The primary objectives of the Specific Plan's storm water management program and drainage concept are:

- To incorporate Low Impact Development (LID) practices wherever feasible;
- To capture, treat, and convey both on and off-site storm water associated with storm events before it enters the Santa Clara River;

CHAPTER 6.0

Infrastructure Development Plan

- To safely convey the 100-year storm flows to a safe point of discharge without flooding any on-site structures;
- To help reduce flooding along Ventura Road by reducing and redirecting all site drainage away from El-Rio Drain;
- To consider the criteria set forth by separate resolutions from United Water Conservation District and Fox Canyon Groundwater Management Agency regarding storm water discharges from the project;
- To minimize impacts to water quality using both mechanical and natural detention, infiltration, and treatment methods in a “treatment train” approach; and
- To meet regulatory and BMP design requirements established by the Ventura County Storm Water Quality Urban Impact Mitigation Plan (SQUIMP), the Regional Water Quality Control Board, and the City of Oxnard.

6.3.3 Approach

Upon re-development of the Specific Plan area, storm water will be directed first to natural bio-filtration and infiltration areas and, if necessary, to below ground mechanical storm water collection and treatment areas before ultimately entering the Santa Clara River. The Specific Plan’s overall drainage concept involves

establishing independent collection and conveyance systems within each drainage sub-area to manage their respective storm flows. These storm flows will be collected, cleansed, and conveyed via a water filtration and storm drain system directly into the Santa Clara River.

The use of a combination of natural Best Management Practices (BMPs), such as Low Impact Development practices, and mechanical stormwater BMPs throughout the various phases of development will provide direct benefits, including:

- Reduction of suspended sediments and sediment associated contaminants via sub-surface detention;
- Natural filtration during over-land flow through vegetated swales;
- Filtration of auto-oriented contaminants via pervious pavement and bio-parking islands; and
- Infiltration and percolation via conventional landscaping and dual-use recreation areas.

The storm water management program will include a number of proven storm water filtration technologies which ultimately will be arranged in a “treatment train” approach. A “treatment train” approach provides multiple opportunities for storm water filtration before the runoff leaves the site.

The following Low Impact Development storm water management solutions shall be considered as part of any final

CHAPTER 6.0

Infrastructure Development Plan

be considered as part of any final drainage design for The Village Specific Plan:

- **Tree Preservation and Planting:** Trees perform a variety of functions that reduce runoff volumes and improve water quality. Leaf canopies intercept and hold large quantities of rainwater on the leaf surface, preventing it from reaching the ground and becoming runoff. Root systems create voids in the soil that facilitate infiltration. Trees also absorb and transpire large quantities of ground water, making the soil less saturated, which allows more storm water to infiltrate. Finally, tree canopies shade and cool paved areas.

To the extent feasible, the existing mature trees within the Specific Plan area shall be preserved and re-used as part of site development.

A tree report shall be prepared by a certified arborist prior to issuance of grading permits to determine the health and economic value of any existing trees to be removed or displaced due to construction within the Specific Plan area, and determine the feasibility for tree relocation. The Planning Manager shall be the review and approval authority over the selection of an arborist. The methodology for tree appraisal value shall be based upon the Trunk Formula Method. The economic appraisal value shall be put back into new tree sizes for the project and shall

be in addition to meeting the City's tree size of 24" box.

The report shall be prepared by a certified arborist and shall follow the format as outlined in "A Guide to the Methods and Procedures for Appraising Amenity Plants," latest edition as published by the International Society of Arboriculture. The report shall include both text and a site plan that clearly labels all trees to be saved, removed or transplanted. The report shall include a discussion of feasible tree mitigations for any tree that cannot be preserved. The report shall also include any mitigation measures required in the EIR.

- **Structural Soils:** Structural Soils are an artificial growing medium that serve the multiple functions of encouraging root growth, satisfying pavement design and installation requirements, and increasing storm water holding capacity. Structural soils should be considered for use wherever feasible, particularly as part of the Specific Plan's parkway and sidewalk landscaping program.
- **Bio-Retention:** Bio-retention facilities are engineered storm water solutions that mimic the natural hydrological cycle and rely on the biological and chemical processes that occur in nature to treat storm water. Bio-retention facilities are designed to utilize soil, vegetation, hardscape elements and other materials to support and enhance the infiltration and bioremediation processes. Bio-

CHAPTER 6.0

Infrastructure Development Plan

retention should be considered for use wherever feasible as part of the Specific Plan's landscaping program.

- **Bio-Filtration:** Bio-filtration facilities include filter strips/planters and vegetated swales, which filter runoff through soils and plant material to remove suspended sediments. The design solutions in this category differ from bio-retention facilities in that their primary purpose is usually to convey storm water rather than to retain or store it. Bio-filtration should be considered for use wherever feasible as part of the Specific Plan's landscaping and/or drainage program.
- **Infiltration:** Infiltration facilities can take a number of forms, including infiltration basins, trenches, sand filters, and French drains, all of which slow and filter runoff, thereby improving the water quality and reducing the volume of runoff leaving a site. Infiltration areas should be considered for use within the Specific Plan's parkways, parks, and landscaped corridors wherever feasible.
- **Permeable Pavement:** Permeable paving systems facilitate infiltration by allowing storm water to soak through the voids in the pavement into an underlying detention basin or a basin that is filled with gravel, a layer of filter fabric, or other filtration media. Permeable pavement areas should be considered for use within the Specific Plan's surface parking lots,

enhanced pedestrian crossings, parks, and landscaped corridors wherever feasible.

- **Mechanical Filtration:** Mechanical filtration systems can be integrated into storm drain systems with the ability to filter everything from trash to metals and organic compounds. Proprietary mechanical treatment within the public right-of-way may only be proposed after other non-proprietary devices approved under the City's current Technical Guidance Manual for Storm water Quality Control Measures have been rejected as infeasible. Maintenance of any propriety mechanical treatment devices within the public right-of-way shall be included in the Community Facilities District.
- **Subsurface Detention:** Underground storm water detention systems can be used as needed beneath parking lots or other appropriate areas to capture and store surface runoff during a rain event. The stored water is then released at pre-development flow rates. If necessary to achieve acceptable off-site drainage volumes, sub-surface detention facilities should be considered for use beneath parks and surface parking lots wherever feasible.

Figure 6.2 provides an example of how the storm water management solutions described above could be applied throughout The Village Specific Plan at the block level.

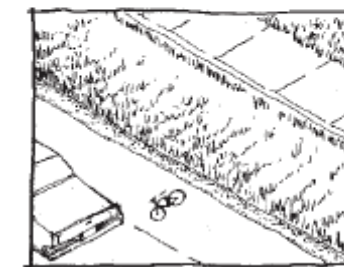
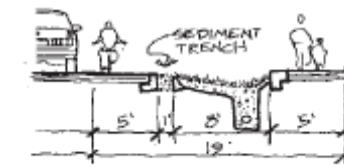
Stormwater Detention Beneath Park



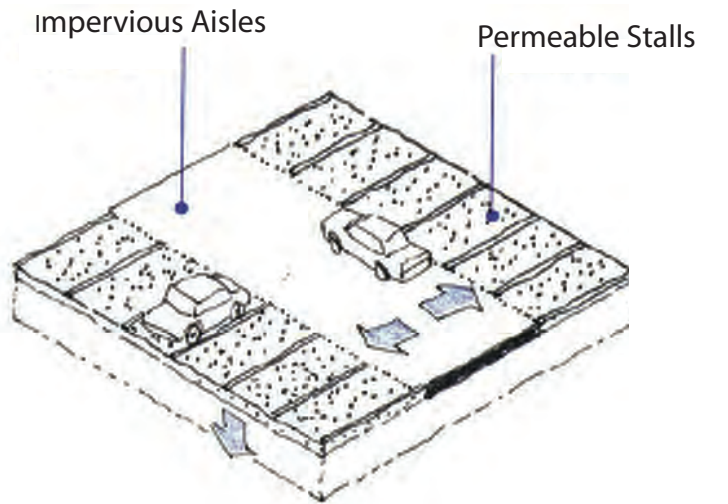
Bio-Retention Used with Roof Drains



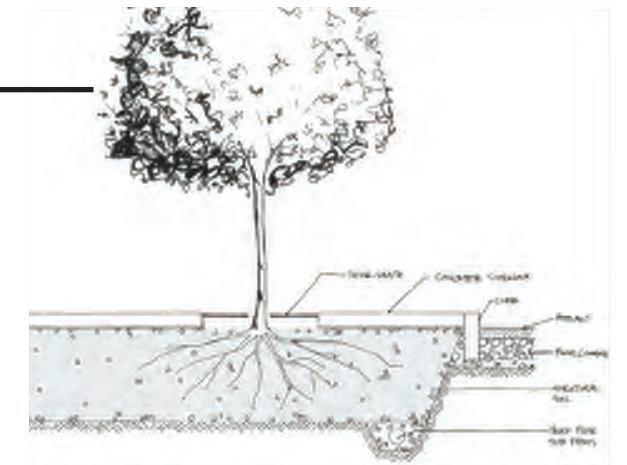
Grass Filter Strips used in Parkways



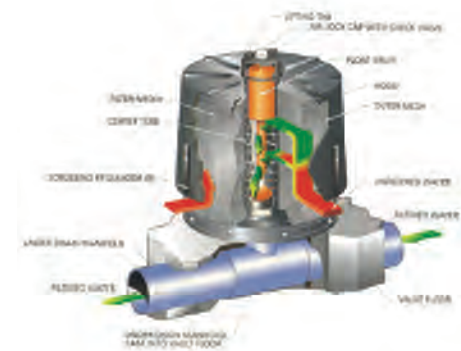
B: GRASS FILTER STRIP



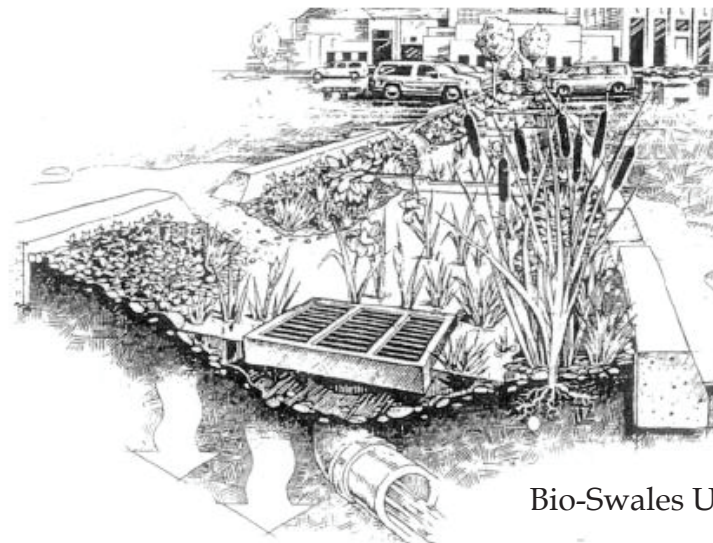
Permeable Pavement used in Surface Parking Areas



Structural Soil System Beneath Planted Parkway



Mechanical Filtration in Storm Drain System



Bio-Swales Used in Planted Median



Potential Stormwater Management Design Solutions

CHAPTER 6.0

Infrastructure Development Plan

6.4 WATER SERVICE

The City of Oxnard's water is currently obtained from the following three sources:

- Imported water from the State Water Project system,
- Water purchased from and delivered by the Calleguas Municipal Water District (CMWD) which in turn purchases water from the Metropolitan Water District (MWD).
- Groundwater obtained from the United Water Conservation District (UWCFD); and local wells operated by the City of Oxnard.

The City blends water from these three sources to achieve an appropriate balance between water quality, quantity, and cost. Both the CMWD and the MWD have assured the City that water supplies will be available in the future.¹

6.4.1 Existing Conditions

The Village Specific Plan is surrounded by the following streets which include existing water pipeline facilities:

- Violet Way – With a 12-inch pipeline under the Union Pacific Railroad right-of-way along the southern boundary of the project and through the development.

- Oxnard Boulevard – With a 12-inch pipeline located from the Esplanade Shopping Center and across Oxnard Boulevard.
- Ventura Road – With an 18-inch pipeline located in Wagon Wheel Road under the 101 Freeway and under Ventura Road.

The Specific Plan's primary sources of water will be Blending Stations No. 3 and No. 1, located to the south, approximately 3.5 miles and 2.85 miles respectively from the Specific Plan area. Of the two, Blending Station No. 3 is the most critical for the project site due to its closer proximity to the site and a 36-inch pipeline under Gonzales Road. These two blending stations combine water from City groundwater wells, Calleguas Municipal Water District, and United Water Conservation District. In the future, the blending stations also will combine groundwater from the City's two desalting facilities.

Water service within The Village Specific Plan Area is currently provided via the 12-inch asbestos cement pipe (ACP) at the intersection of Spur Drive and Oxnard Boulevard, directly across from the Esplanade Shopping Center.² At this time, existing 8-inch and 12-inch water lines traverse the site beneath the existing roadways. These pipes ultimately connect off-site to existing water pipelines located in Violet Way.

¹ City of Oxnard 2020 General Plan. Public Facilities Element, Page VII-5.

² Kennedy Jenks. Wagon Wheel Development Infrastructure Review. April 2007

CHAPTER 6.0

Infrastructure Development Plan

6.4.2 Proposed Conditions

As part of future site re-development consistent with The Village Specific Plan, the existing water infrastructure will be abandoned and/or removed and replaced with a new looped water service system as required by the City of Oxnard Public Works Department.

6.4.2.1 Water Demands

The 1,500 residential units, 50,400 square feet of commercial, and associated landscaping proposed as part of The Village Specific Plan is estimated to need 640 Acre-Feet/Year of water³. The Oxnard Village Water Supply Assessment concluded that there will be sufficient water supplies to serve all development proposed as part of this Specific Plan⁴. To ensure water supply availability, the pace of development shall not occur more rapidly than as set for below:

- Phase 1 (up to 600 apartment units and up to 50,400 square feet of commercial space) will be completed for occupancy from the date of approval through December 31, 2011; Phases 2 and 3 (up to an additional 693 apartment or townhouse units) will be completed for occupancy from January 1, 2012 through December 31, 2013; and Phase 4 (207 residential high rise units) will be completed for occupancy after January 1, 2014.

³ Oxnard Village Water Supply Assessment. June 2007, prepared by Kennedy Jenks Consultants

⁴ Oxnard Village Water Supply Assessment, May 2008, prepared by Kennedy Jenks Consultants

6.4.2.2 Water System Pipeline Design

As stated above, all existing water service infrastructure will be abandoned and/or removed as part of Specific Plan buildout. The following on and off-site water infrastructure improvements will support full buildout of The Village Specific Plan:

A new 16-inch water pipeline under Main Street will traverse the site from the existing 12-inch pipeline under Oxnard Boulevard and connect to the existing 18-inch pipeline located under Ventura Road.

- New 8 to 12 inch domestic water pipelines will be located beneath the neighborhood streets to serve the commercial and residential uses.
- Appropriately sized lateral water lines will be looped through the building sites to provide domestic water and Fire Department emergency water flows to the development areas. Figure 6.3 illustrates the conceptual design of water, sewer, and storm drain improvements within The Village Specific Plan Area.

6.4.2.3 Fire Department Flows

To ensure that the Specific Plan Area and other higher intensity uses currently existing and planned within the northern Oxnard region continue to receive the required Fire Department water flow of 4,500 gallons per minute

CHAPTER 6.0

Infrastructure Development Plan

(gpm), additional regional improvements to the City's water infrastructure are required, including the following:

- Installation of a 20-inch pipeline in Ventura Road from Gonzales Road north to the Specific Plan's Ventura Road entrance. This improvement requires the extension of approximately 8,400 feet of pipeline.
- Installation of a 20-inch pipeline along Oxnard Boulevard from Gonzales Road to the Specific Plan's entrance on Oxnard Boulevard. This improvement will tie into the existing 36-inch pipeline beneath Gonzales Road and will require the extension of approximately 6,600 feet of pipeline.
- Each high rise residential building will require the installation of a separate Underwriters Laboratory rated water pump to achieve the required Fire Department water flows.

It is anticipated that the above described improvements can be constructed at a cost of approximately \$5-6 million. Once completed, the improvement will benefit additional properties within the vicinity of The Village Specific Plan and thus will be subject to a cost sharing and/or reimbursement agreement.

Prior to issuance of building permits for any phase of The Village Specific Plan, the City will determine the developer's responsibility for construction and/or financial contribution towards the above

referenced regional water improvements.

6.5 WASTEWATER SERVICE

Wastewater treatment within The Village Specific Plan area is provided by the City of Oxnard Wastewater Treatment Plant (OWTP). The City is currently in the process of expanding the OWTP to increase the current design capacity of 22.6 million gallons per day to 39.6 million gallons per day of average dry weather flow.⁵

6.5.1 Existing Conditions

Currently, wastewater generated by existing uses within the Specific Plan area is conveyed by 8-inch and 10-inch pipelines beneath the existing roadways or building pads. Ultimately, these pipelines connect to a 15-inch vitrified clay pipe (VCP), which crosses beneath Ventura County's El Rio Drain and the Union Pacific Railroad at the southern portion of the site. South of the channel crossing, the size of the sewer line is reduced to a 12-inch pipe which continues southward through the Southbank residential neighborhood beneath Grapevine Drive. This wastewater pipeline reaches the City's Ventura Road Trunk Sewer line pump station at the intersection of Vineyard and Ventura Road. From there, a pump station forces all wastewater flows south towards the City's Wastewater Treatment Plant.

⁵ City of Oxnard 2020 General Plan. Public Facilities Element. Page VII-3.

CHAPTER 6.0

Infrastructure Development Plan

6.5.2 Proposed Conditions

The Village Specific Plan will require wastewater improvements, including abandoning and/or removing the existing on-site 8-inch and 10-inch wastewater pipelines north of the El Rio drain and replacing them with new 8-inch and 12-inch sewer lines. The new main lines are proposed beneath Main Street and the other public roadways.

The first phase of development within the Village Specific Plan proposes to connect to the existing wastewater infrastructure serving the property. This involves connecting to the existing 10-inch sewer line at the Spur Drive/Oxnard Boulevard intersection and the existing 12-inch sewer line which crosses under the El Rio Drain and runs beneath Grapevine Drive. The existing sewer system can be used for Phase 1 if a sewer study demonstrates that capacity exists in the downstream system including Lift Station 23. Based on calculations provided by the developer, the City will determine when additional offsite downstream improvements are required.

Subsequent development phases will require construction of the remaining on-site wastewater improvements, including the construction of an 8-inch sewer line beneath on-site public streets and all appropriately sized laterals. Off-site sewer improvements are also required and include the payment of fees towards the construction of a new sewer line beneath Ventura Road and the upgrading of sewer Lift Station 23.

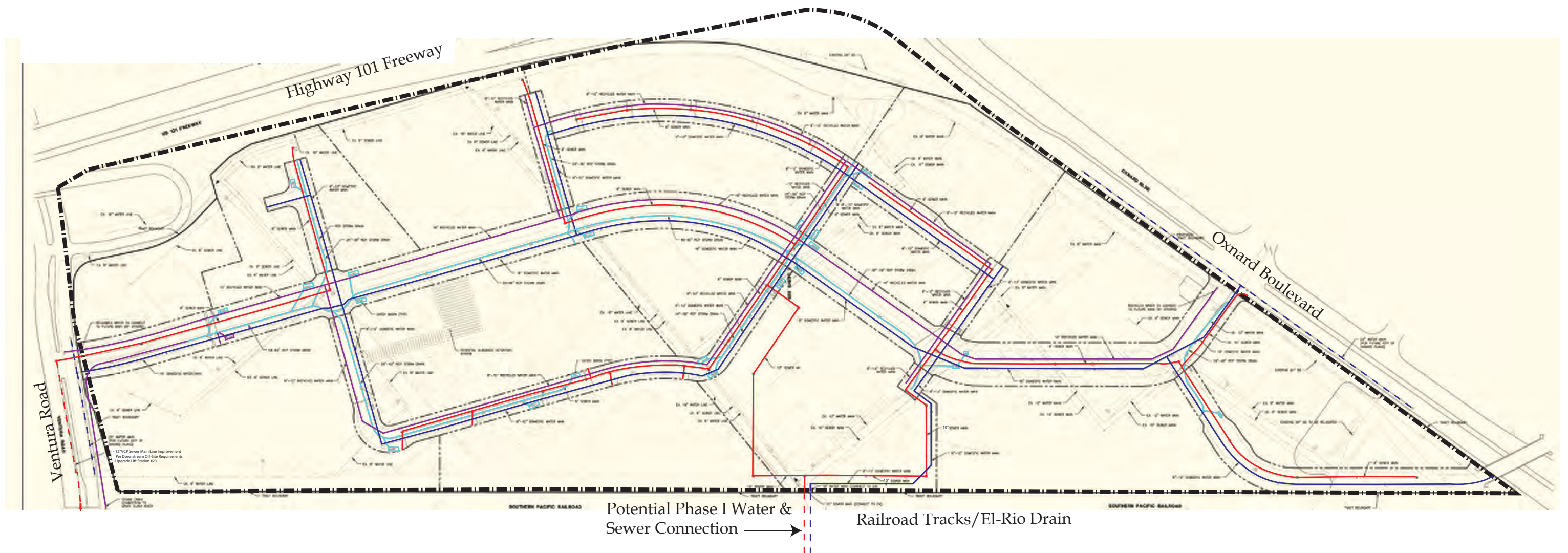
If the City cannot construct the off-site improvements consistent with the developer's construction schedule, then the developer may elect to install the off-site improvements subject to a reimbursement agreement for those costs that are considered a City responsibility. The details of such a reimbursement agreement would be discussed in formal conditions of approval or the Development Agreement. Figure 6.3 illustrates the anticipated configuration and size of all proposed wastewater facilities.

6.6 RECYCLED WATER

Recycled water infrastructure is currently being installed within Ventura Road by the developers of Riverpark in conformance with the City's Groundwater Recovery Enhancement and Treatment (GREAT) Program. Although the City estimates that recycled water will not be available for distribution until mid-2010⁶ as part of the City's GREAT program, The Village Specific Plan proposes to install a 16-inch "backbone" recycled water pipeline within Main Street, and 8 and 12 inch recycled water pipelines beneath other on-site roadways.

The 16-inch "backbone" recycled water pipeline would allow future uses to connect into the recycled water system once it is operational. Once recycled water is available, this system would be used to irrigate all common landscaped areas within The Village Specific Plan.

⁶ Kennedy Jenks. Recycled Water Facilities Plan. 2007



Legend	
	The Village Specific Plan Boundary
	8-16" Backbone In-Tract Water System
	Potential Regional Improvement to Water System Per City Master Plan (20")
	8-16" Backbone In-Tract Recycled Water System
	8-12" Inch Backbone In-Tract Wastewater System
	Potential Regional Improvement to Wastewater System Per City Master Plan(12")
	30-42" Backbone In-Tract Storm Drain System

Preliminary Backbone Infrastructure Plan

CHAPTER 6.0

Infrastructure Development Plan

6.7 DRY UTILITIES

Electric, gas, and telecommunication infrastructure will be installed to serve the Specific Plan. These “dry” utilities will be located within underground conduits and vaults in the public or private street corridors in general conformance with the phasing of the project. All existing dry utility infrastructure will be replaced as part of Specific Plan buildout. To the extent feasible, all above ground utilities are proposed for removal and will be placed underground. Consultations with all appropriate utility agencies to determine the extent of dry utility improvements will be required prior to and during the design of final improvement plans.

6.8 ROADWAY DESIGN

6.8.1 Objectives

Roadway design within The Village Specific Plan was influenced by two interrelated objectives:

- To calm vehicle speeds which creates a safe environment for pedestrians, bicycles, automobiles, personal electric cars, service and delivery vehicles, and public transportation; and
- To provide an efficient roadway and pedestrian network connecting The Village Specific Plan with surrounding uses.

The Village Specific Plan’s circulation design meets the objectives described above by:

- Proposing streets intentionally designed to establish a safe and efficient vehicle, bicycle, and pedestrian network, while at the same time creating a high-quality environment consistent with the proposed residential and mixed use architectural themes;
- Establishing a circulation system designed to ensure pedestrian and bicycle safety over maximum vehicle traffic speeds;
- Providing pedestrian, bicycle and vehicle linkages to major transportation corridors, shopping destinations, and transit within the City of Oxnard; and
- Encourage the use of public transportation by providing a multi-modal sub-transportation center, which will add to the range of transit services available in the City of Oxnard and the northern Oxnard region.

6.8.2 Regional and Local Vehicular Circulation

The Specific Plan’s vehicle circulation network includes a hierarchy of streets ranging from regional highways to privately maintained alleys. U.S. Highway 101, Oxnard Boulevard and Ventura Road provide the regional access to the Specific Plan area and

CHAPTER 6.0

Infrastructure Development Plan

beyond. These regional arterials form the Specific Plan's northern, eastern and western borders, respectively. Internally, circulation is established through the use of Main Street as the central thoroughfare. The neighborhood streets intersect with Main Street, forming multiple points of access to and from the residential neighborhoods. The inter-connected street network permits the dispersion of traffic among the various internal streets, which allows the streets to be narrower while still meeting the City's traffic flow and emergency vehicle requirements. The external and internal vehicle circulation network is illustrated in Figure 5.2.

Figures 5.3 through 5.6 and Figures 5.16 through 5.18 illustrate the conceptual roadway cross-sections envisioned within The Village Specific Plan. The cross-sections establish the public and private realm, parkway widths, sidewalk widths, median widths, parking lane widths, bicycle lane widths, and other typical roadway design specifications. It is important to note that these cross-sections are conceptual and may be modified slightly as more detailed engineered designs are prepared.

However, the following roadway design characteristics shall remain part of any engineered roadway design concept.

The essential characteristics include:

1. All streets shall have pleasant sidewalks, and tree lined parkways separating pedestrians

from vehicle travel lanes. In addition, all streets have on-street Class II or III bicycle lanes,

2. All streets shall have curbside parking (either parallel or angled). Exceptions to this shall be limited to curbs near intersections, fire hydrants emergency access points, within alleys, and at designated transit stops.
3. The number and width of lanes on each street shall not be greater than what is necessary to establish a safe travel environment for pedestrians, sufficient access for emergency vehicles, and efficient yet calm vehicle traffic flow.
4. All critical intersections, such as the intersections of Main Street and Streets "A", "B", "C", "D", and "E", shall include some form of traffic calming feature, such as traffic circles, enhanced crosswalks, mid-block crossings and/or bulb-outs. Proposed traffic calming mechanisms are illustrated on Figure 5.7.

6.8.3 Bicycle Circulation

The bicycle circulation network proposed as part of The Village Specific Plan includes a Class I off-street bicycle/pedestrian pathway along the project's northern frontage adjacent to U.S. Highway 101, and Class II on-street bicycle lanes along the project's Oxnard

CHAPTER 6.0

Infrastructure Development Plan

Boulevard and Ventura Road frontages. These pathways provide a regional connection between Oxnard Boulevard, Ventura Road, and the Santa Clara River.

Class II bicycle lanes are also proposed along the residential section of Main Street, and Class III bicycle lanes are proposed along the commercial section of Main Street and along all Neighborhood Streets.

The off-street and on-street bicycle pathways will interconnect the residential neighborhoods with the mixed use areas. The pathways will also connect to Riverpark, the Esplanade Center, and the City's planned Santa Clara River Trail. Figure 2.5 illustrates the proposed bicycle pathway network.

6.9 PARKING

6.9.1 Objective

Parking regulations and standards within The Village Specific Plan are based primarily upon the following goal:

- Utilize a combination of shared and non-shared parking to provide the requisite number of parking spaces for all uses, while reinforcing the project's mixed-use and pedestrian oriented character.

One of the ways this goal can be accomplished is by minimizing vehicle trips and parking demand by providing a mix of uses within walking distance of each other and within walking distance

of mass transit. A well planned mixed-use community reduces the area required for parking and roadways, which ultimately permits re-investment into the public realm (building architecture, pedestrian landscaping, and recreational facilities), which in turn maintains the village environment.

The overall parking plan is based on five major goals:

- 1. Minimizing the Acreage Paved for Parking:** Provide the most efficient use of space for parking and avoid a "sea of parking" by utilizing shared parking principles where appropriate. Conveniently locate on-street, off-street, and structure parking throughout the Specific Plan.
- 2. Vehicle Trip Reduction:** Parking space demand can be reduced by substituting vehicle trips with pedestrian trips created by The Village's pedestrian-oriented land use plan (Figure 2.4). To further reduce trips, the Specific Plan promotes the use of alternative forms of transportation through the creation of the Wagon Wheel sub-transportation center (Planning Area 19). The transportation center is planned to give residents and visitors convenient access to Personal Electric Vehicles (PEVs), Segways, Car Sharing, bicycles, van-pools, express bus service, neighborhood shuttle services, and Metrolink service. Section 6.10 Transportation Demand

CHAPTER 6.0

Infrastructure Development Plan

Management provides a more complete description of this program.

- 3. “Park Once” Strategy:** Parking for the Mixed-Use, High Rise, Transit Center, and Very High Density Planning Areas (Planning Areas 16-21) will be part of a “Park Once” shared parking district. This strategy allows a visitor to park, visit a shop, office, and/or residence without having to move their car. This strategy reduces the total number of parking spaces needed, and eliminates un-necessary auto trips. Convenient parking locations combined with strong pedestrian orientation and a mix of uses makes this strategy a viable part of The Village Specific Plan.
- 4. Shared Parking District - Planning Areas 16-21:** Figure 6.4 identifies the location of the shared parking district. Within the designated shared parking areas, residents, visitors, shoppers, business owners, and their employees are permitted to share on and off-street surface parking, and structure parking during off-peak periods.

For example, during the evening, parking spaces typically dedicated for retail uses are largely empty and thus can be “shared” or filled with residents who are parked overnight. Conversely, residential spaces are largely empty during daytime peak retail periods and so can be shared or filled with shoppers who are parked temporarily. Parking within the

Shared Parking District will be managed as part of a Transportation Management Association. A managed supply of parking will result in a reduction in the number of built parking spaces, thus freeing up additional area for landscaping, while still maintaining the ability to find parking at all times. It is important to note that the shared parking concept is based upon reasonable parking ratios for the commercial retail uses and the residences. Thus, parking will be available during peak periods for each type of use.

- 5. On-Street Parking:** The Village Specific Plan proposes unrestricted on-street parking as a way to meet a portion of the parking demand for residential and commercial uses. On-street parking for residents, visitors and customers, both by calculation and by design, can contribute to pedestrian safety, reduced vehicle speeds, and efficient use of parking supply.

6.9.2 Parking Regulations

The Village Specific Plan includes two types of parking areas, shared and non-shared. Non-shared residential parking spaces are provided within the High Density Residential and Live Work Planning Areas (Planning Areas 1-14). Shared parking is planned for the Mixed Use, High Rise, Transit Center, and Very High Density Planning Areas (Planning Areas 16-21).

CHAPTER 6.0

Infrastructure Development Plan

The shared parking areas have been established based upon the City of Oxnard Zoning Code requirement that shared parking spaces shall only be located within 500 feet of any use served. Thus, the shared parking areas are all located within 500 feet of the Mixed-Use, High Rise, TC Overlay and the Very High Density Residential Planning Areas. The shared parking spaces are distributed among on-street parallel and angled parking, off-street parking lots, and off-street parking structures. Figure 6.4 specifies the location of all shared and non-shared parking areas within the Specific Plan.

- **Parking Space Size:** The size of all parking spaces will comply with the standards established by the City of Oxnard Municipal Code.
- **Shared Parking Ratios:** Within the shared parking areas, residential parking demand ratios of 1.5 spaces per owner-occupied condominium unit and 1.2 spaces per unit for rental apartments were recommended by Nelson-Nygaard, as part of the Specific Plan's preliminary Shared Parking Study. A commercial parking demand ratio of 2.5spaces/ 1,000 square feet of commercial development was recommended as part of this same study. The ultimate goal of the shared parking approach is to permit flexibility in the municipal parking standards in favor of a system where the private sector develops parking to meet only the needs of development without over-building parking supply. The preliminary Shared Parking Study prepared by Nelson-Nygaard and Associates concluded that approximately 1,728 parking spaces (including 154 "shared" spaces) will be needed within the Shared Parking District to meet the parking demand for residents,

guests, shoppers, business owners, and their employees.

- **Non-Shared Residential Parking Ratios:** Within the High Density Residential Planning Areas (Planning Areas 4-13), an overall parking ratio of 2.5 spaces per residential unit was used to calculate the number of parking spaces required for both residents and guests. These non-shared spaces are provided in a combination of two-car garages for each residential unit plus on-street parallel parking, and off-street parking areas for guest parking. The total number of non- shared spaces available to the High Density Residential Planning Areas will likely exceed the City's standard parking requirements. Nevertheless, this Specific Plan permits the establishment of a residential permit parking program within the High Density Residential Planning Areas, if the City determines that spillover parking is creating parking shortages. Establishment of such a program would be subject to review and approval by the Planning Manager.

CHAPTER 6.0

Infrastructure Development Plan

6.9.3 Parking Area Location, and Type

Parking Locations for High Density Residential Planning Areas: Parking for the High Density Residential Planning Areas (PAs 2-13) will be provided enclosed garages for each residential dwelling accessed from an alley. On-street parking is prohibited within the alley. Guest parking for High Density Residential Planning Areas will be located within off-street surface parking areas and parallel on-street parking areas. Guest parking shall be located within an acceptable distance of the surrounding residences, per the City of Oxnard Zoning Ordinance.

Parking Locations for Mixed-Use Planning Areas: Parking for the Mixed-Use Planning Areas (PAs 17, and 18) will be provided in on-street and off-street surface parking areas, subterranean or on-grade garage parking, under a shared parking arrangement.

- **Parking Locations for Very High Density Planning Area:** Parking for the Very High Density Planning Area (PA 21) will be provided by structured parking or a combination of on-grade tuck under and surface parking subject to review and approval by the City of Oxnard Planning Department. Any on-site tuck under and surface parking shall be appropriately screened from the street using landscaping and building forms. In either scenario, additional guest parking will be provided in on-street parking areas. On-street parking shall be subject to a shared parking management program.

- **Parking Locations for High Rise Planning Areas:** Parking for the High Rise Tower Planning Area (PA 16 and 20) will be located in two parking structures under a shared parking arrangement. The majority of resident and guest parking will be provided within the parking structures, and on neighboring streets under a shared parking arrangement.

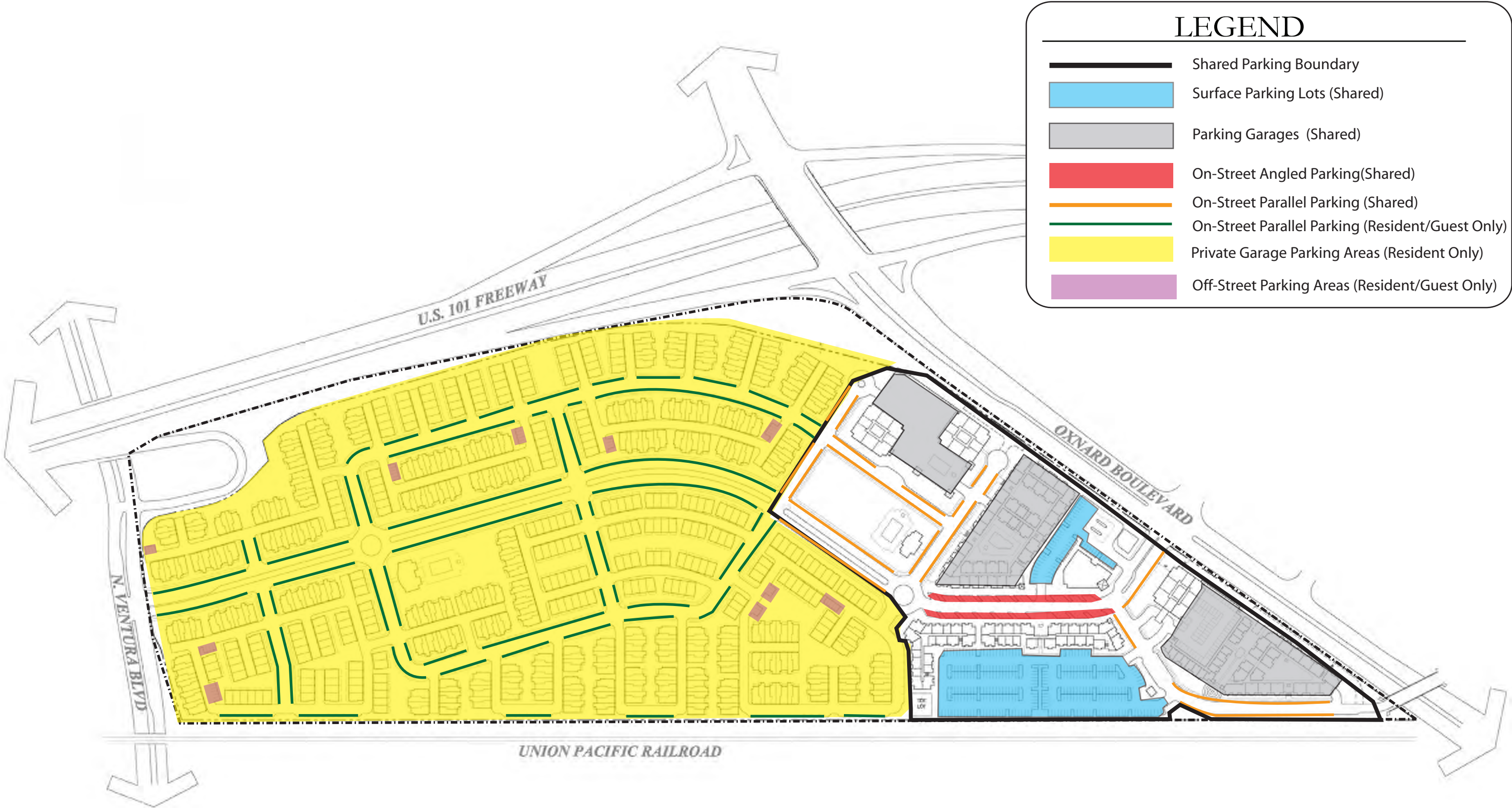
6.9.4 Parking Management Plan

A Shared Parking Management Plan will be prepared prior to issuance of building permits for any development project within the shared parking district as

CHAPTER 6.0

Infrastructure Development Plan

depicted on Figure 6.4. The plan shall include: (1) a description of shared parking arrangements and confirm that a shared parking arrangement will adequately handle on-site commercial and residential parking demand, (2) a detailed parking management strategy, (3) discuss ways in which on-site parking areas could be expanded depending on future need and, if needed, the mechanisms for funding future parking expansion.



*Note: Parking lot areas are subject to change during final design

CHAPTER 6.0

Infrastructure Development Plan

6.10 TRANSPORTATION DEMAND MANAGEMENT (TDM)

6.10.1 Introduction

The Village Specific Plan is designed to promote the use of alternative local and regional modes of transportation in-lieu of continuous private automobile use. Regardless of the specific service or mode, increasing the use of alternative forms of transportation is an essential element in meeting local and regional transportation planning objectives (such as improving traffic flow), environmental objectives (such as improving air quality), and social objectives (such as improving quality of life and increasing the affordability of housing).

The goals of the Specific Plan's Transportation Demand Management (TDM) program are to:

- Reduce the number of vehicle trips generated by existing and future uses within northern Oxnard;
- Increase awareness and participation in the TDM program by encouraging existing and future developments to implement some or all of the TDM concepts;
- Increase transit ridership and the use of alternative transportation modes within the City of Oxnard as a whole.

6.10.2 TDM Program Components

The Wagon Wheel Sub-Transportation Center is envisioned as a place where

one can conveniently access mass transit and alternative modes of transportation in northern Oxnard. Local and regional transportation alternatives will be provided within the Transit Center, which is conveniently located in the southeastern portion of the Specific Plan, adjacent to the Union Pacific Railroad/Metrolink Line and along the existing Gold Coast Transit and Vista bus service routes. Although the transit facility or transit "hub" is proposed within The Village Specific Plan, the TDM programs are also planned to serve the surrounding communities and businesses in northern Oxnard, such as Riverpark, Channel Islands Center, Southbank, the El Rio West neighborhoods, as well as the Esplanade Shopping Center, and the Topa Financial Plaza.

The Wagon Wheel Sub-Transportation Center and the TDM program components are proposed for construction/implementation as part of development Phase III, as outlined in Section 8.5. The Developer will be responsible for implementation of the program prior to issuance of a building permit for the 750th residential unit, or upon verification by the City that sufficient transit demand exists.

Transit services envisioned within the Wagon Wheel Sub-Transportation Center include:

- Express morning and afternoon shuttle service to the Oxnard Transportation Center, and Traditional bus service to other local

CHAPTER 6.0

Infrastructure Development Plan

and regional destinations throughout Ventura County;

- 50 designated Park-n-Ride spaces (expandable if necessary);
- Go-Point™ mobility center, which will provide residents with opportunities to conveniently rent a variety of Personal Electric Vehicles, Segways, and bicycles;
- Car Sharing;
- Transit oriented commercial uses such as a small coffee shop and/or newsstand;
- VPSI Van Pool Service to major employment centers such as Santa Barbara, Amgen in Thousand Oaks, and Warner Center; and
- A future Metrolink Stop.

Other components planned as part of the Northern Oxnard TDM Program include:

- **Introductory Transportation Information Packet:** provided to all residents and employees, outlining TDM programs, routes, schedules, carpools/vanpools, shuttle/bus service maps, menu of incentives, etc.

Carpool/Vanpool/Ridematching

Services: This program would match residents and employees in Northern Oxnard in carpools and vanpools to reduce drive alone trips. A Guaranteed Ride home service would provide reimbursement for

immediate transportation home via taxi or other similar mode to those in an emergency.

- **Subsidized Transit Pass:** Transit passes would be purchased in bulk so that bus and rail passes could be provided for residents and employees within northern Oxnard. These passes typically provide unlimited rides on local or regional transit for low monthly fees.
- **Priced Commercial Street Parking:** Multi-Spaced parking meters are planned along portions of Main Street with rates calibrated to ensure an 85% occupancy rate. This will provide a high level of convenience for parkers, largely eliminates circling for parking, and will help ensure turnover of the most convenient curbside parking spaces and availability for customers.
- **Parking Cash-Out:** Parking cash-out provides an equal transportation subsidy to employees who ride transit, carpool, vanpool, walk, or bicycle to work. Employees can be offered financial incentives such as free transit passes or a cash bonus to carpool, vanpool, bicycle, or walk, thus decreasing the demand for parking and ultimately reducing traffic congestion.

Recent transportation case studies show that a successful TDM program can significantly reduce vehicle traffic. Moreover, the overall pedestrian oriented design aspects of The Village

CHAPTER 6.0

Infrastructure Development Plan

may contribute to the “captive market effect,” where a high density, walkable project designed with a greater mix of uses will attract people with fewer cars and require fewer car trips on a daily basis. The project itself permits the establishment of businesses that may carry essential goods and services within a comfortable walking distance of residential dwellings.

6.10.3 Long-Term TDM Program Management

A Transportation Management Agency (TMA) is envisioned as the entity responsible for managing and funding the TDM Program. Typically, a TMA’s responsibilities include funding oversight, information dissemination, overall TDM program management and development, marketing, and management of incentive packages. TMA’s are often legally constituted (similar to a Homeowner’s Association) and frequently led and funded by the private sector in partnership with the public sector to solve transportation problems. Residents, business owners, city officials, and developers of projects within Northern Oxnard are envisioned as TMA members. A TMA, or equivalent entity should be established prior to implementation of the Northern Oxnard TDM program.

The developers of The Village Specific Plan are committed to the following TDM implementation strategies:

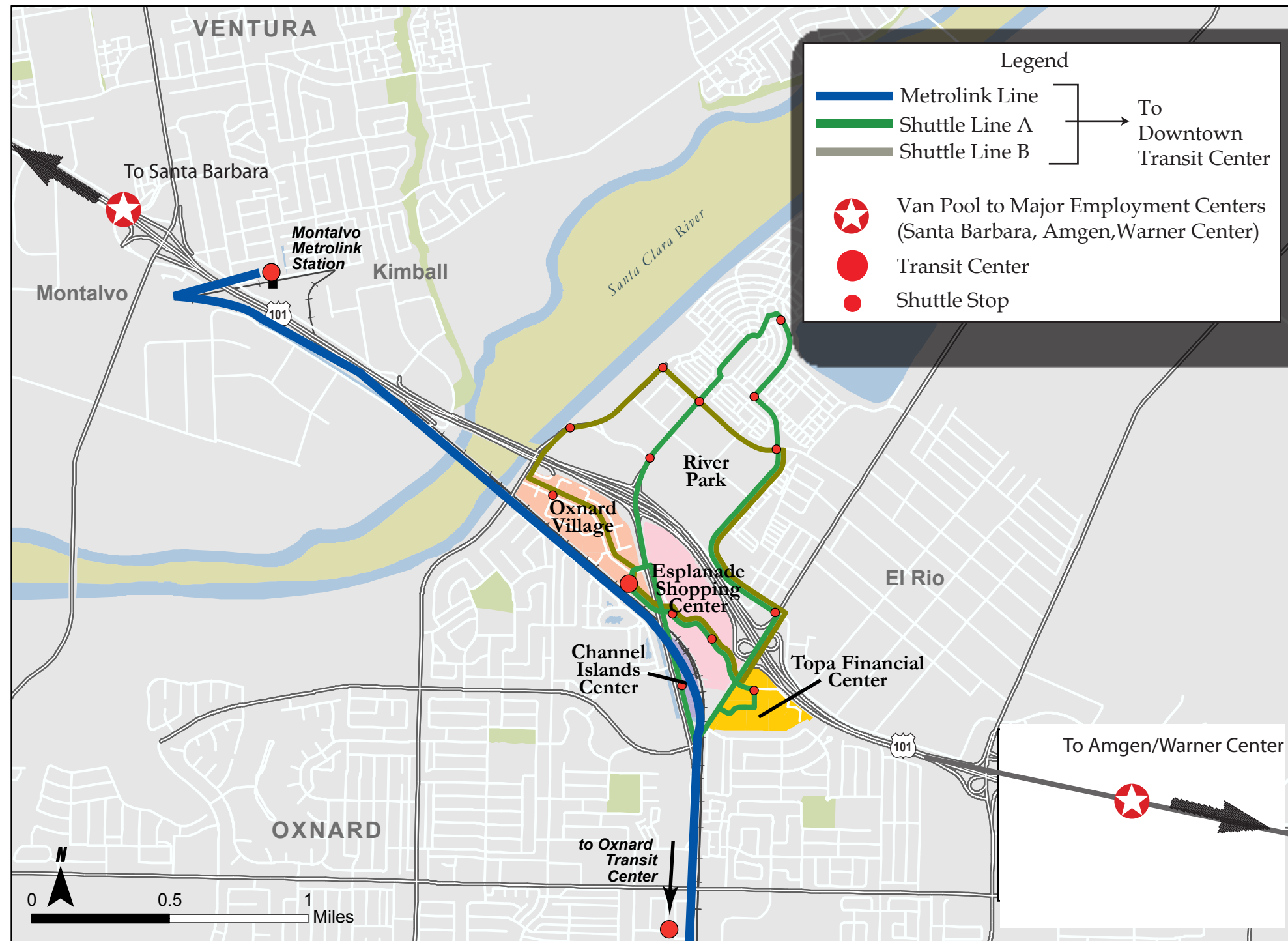
- Work closely with key neighborhoods, business owners,

the City of Oxnard, and developers within the Northern Oxnard TDM District to develop an efficient and financially feasible TDM program;

- Work closely with the transit service providers, and the City of Oxnard to assure that the type, routes, location of transit stops, information signage and related facilities will meet the needs of the people who live, work, and shop within Northern Oxnard, and;
- Utilize similar standards of urban design and architectural quality for the transit stops, signage, shelters, and any other structures.

Figure 6.5 identifies important aspects of the Northern Oxnard TDM Program, illustrates the intended service areas, and provides a preliminary estimate of the proposed transit routes. Please refer to Figure 7.1 for a description of important design elements for the Wagon Wheel Sub-Transportation Center.

Northern Oxnard Mobility Zone Study



Legend

- Metrolink Line
- Shuttle Line A
- Shuttle Line B

} To Downtown Transit Center

- ★ Van Pool to Major Employment Centers (Santa Barbara, Amgen, Warner Center)
- Transit Center
- Shuttle Stop

The Village Specific Plan will help facilitate the establishment of the Northern Oxnard “Mobility Zone,” and Northern Oxnard TDM Program, where convenient access to alternative “travel choices” will be provided and will to help significantly reduce traffic within northern Oxnard.

Planned TDM components include:

- Gold Coast Transit/Vista bus service
- Metrolink Shuttle (Bus Rapid Transit);
- On-demand rental of Segways/electric cars/bicycles
- Car sharing
- Van pool service
- Park and ride
- Potential for future Metrolink stop

The benefits of successfully establishing such a program are many, but some of the more notable include:

- Improved Traffic Flow and decreased congestion along the 101 Freeway and area intersections;
- Improved Air Quality through reduction in automobile usage;
- Increased Ridership of Metrolink, Bus Rapid Transit;
- Improved Health and Safety through reduced vehicle speeds and interconnected bicycle and pedestrian pathways;
- Increased Open Space through a reduction in vehicle parking;

Nelson Nygaard consulting associates

GIS Data Source: City of Oxnard



The Village

SPECIFIC PLAN

Chapter 7

Design and Architectural
Standards and Guidelines

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.1 INTRODUCTION

This Chapter describes the detailed standards and other key design elements which collectively establish the desired character of the buildings within The Village Specific Plan's various planning areas. The main categories include massing for commercial and residential portions of the buildings and their relative transparency, upper story fenestration, colors, materials, details, architectural integration of lighting, proper placement of mechanical units and service areas, sidewalk enhancements (trees, planters, paving) and general wayfinding design concepts. Throughout this Chapter, required design elements are indicated by utilizing "shall" or other similar language; guidelines are indicated by utilizing "should," or other similar language.

7.1.1 Mixed Use Building Massing and Organization

1. Buildings in the Village Mixed Use Planning Areas shall provide well-crafted architectural details consistent with a high density village design concept. Aspects of attractive older buildings, such as materials, colors, proportions, window types, cornices, and overall composition should be reflected. By strongly relating to the best examples of the design elements listed above, new buildings will coordinate while adding interest and variety.

2. The building mass and/or façade composition shall vary to promote a scale of building compatible with a "main street" theme. Building façade modulation can be created in a number of ways, such as changes in roof line and window groupings, projecting or recessing wall surfaces, and/or placement of piers and pilasters.



Example of Appropriate Mixed Use Building Massing and Orientation

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.1.2 Special Architectural Features

Features such as gables, turrets, towers, and loggias shall be used to accent buildings at major street corners, at the terminus of a street corridor, alley, or pedestrian way, and at other highly visible locations.

7.1.3 Façade Composition

1. Every building shall have a defined base, a clear pattern of openings and surface features, a recognizable entry, and an interesting roof line.

- a) Building Base - The building base may be as simple as a small projection of the wall surface and/or a different material or color. It may be created by a heavier or thicker design treatment of the entire ground floor for a building of two or more floors, or by a setback of the upper floors.
- b) Pattern of Features - Windows, wall panels, pilasters, building bays, and storefronts should be based on a module derived from the building's structural bay spacing. Features based on this module should be carried across windowless walls to relieve blank, uninteresting surfaces.
- c) Building entrances shall be prominent and easy to identify. Main building entrances shall be easily identifiable. At least one of the following treatments is

recommended for building entrances:

- d) Marked by a taller mass above, such as a tower, or within a volume that protrudes from the rest of the building surface;
- e) Located in the center of the facade, as part of a symmetrical overall composition;
- f) Accented by architectural elements, such as columns, overhanging roofs, awnings, and ornamental light fixtures;
- g) Marked or accented by a change in the roof line or change in the roof type.



Appropriately Enhanced Building Entry

CHAPTER 7.0

Design and Architectural Standards and Guidelines

2. Corner buildings shall provide prominent corner entrances for shops and other activity generating uses.



Example of Prominent Corner Entry

7.1.5 Roofs and Roof Lines

1. Roofs and roof lines shall provide visual interest from streets below and residential uses above, and should complement the overall facade composition. Flat roofs are acceptable if a strong, attractively detailed cornice and/or parapet wall is provided.
 - a) Parapet walls are recommended; they should have a distinct shape or profile, e.g. a gable, arc, or raised center.
 - b) “Commercial Mansards,” (wrap-around roofing panels) that do not enclose a habitable floor, should not be used.
 - c) Mansards should only be used when emulating a traditional building style that typically employs mansard roofs.
 - d) Accent elements, such as flags, cut-out openings, grilles and latticework, ornamental medallions or building numbers are recommended.
 - e) Mechanical equipment on rooftops should be screened behind a architecturally integrated parapet element.

7.1.6 Storefronts

1. Base - a panel of tile, stone or other special building material as described in Section 7.1.9 shall be utilized to accent display windows. Base materials shall be the same or “heavier” when visually compared to the surrounding walls.



Example of Appropriate Storefront Treatment

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- a) Brick and wood should only be used if the rest of the wall surface is the same material; neither material should be used exclusively.
 - b) Ceramic tile is frequently used as a storefront base. Dark tile with light stucco is an effective combination. Different colors and sizes of tile may be used for decorative effect.
2. Display Windows designed with large pane windows encompassing a minimum of 60% of the storefront surface area are recommended. Where privacy is desired for restaurants, professional services, etc., windows should be divided into smaller panes.
 3. Clerestory Windows are horizontal panels of glass between the storefront and the second floor. They are a traditional element of "main street" buildings, and are recommended for all new storefronts.
 4. Recessed Entries are recommended as a traditional element of the main street storefront. The following treatments are recommended:
 - a) Special paving materials such as ceramic tile.
 - b) Ornamental ceilings such as coffering.
 - a) Decorative light fixtures.
5. Doors shall be significant and well detailed. They should match the materials, design and character of the display window framing. Aluminum frame doors are not recommended.
 6. Cornices should be provided at the second floor (or roof lines for a one-story building) to differentiate the storefront from upper levels of the building and to add visual interest; this design also allows the storefront to function as the base for the rest of the building.

7.1.7 Side and Rear Building Facades

4. Side and rear building facades shall have a level of trim and finish compatible with the front façade, particularly if they are visible from the streets and parks, or are adjacent to parking areas or residential buildings.

CHAPTER 7.0

Design and Architectural Standards and Guidelines



Example of Appropriate Rear Building Façade Treatment

7.1.8 Blank Wall Areas

2. Blank wall(s) without architectural treatments are not permitted. Surface reliefs, and/or architectural murals and other surface enhancements are required.

7.1.9 Wall Surface Materials

2. If the building mass and pattern of windows and doors is complex, simple wall surfaces are preferable (e.g. stucco). If the building volume and the pattern of wall openings are simple, additional wall texture and articulation should be employed (e.g. bricks or blocks, rusticated stucco, ornamental reliefs). In both cases, pilasters, columns, and cornices should be used to add visual interest and pedestrian scale.
3. The palette of wall materials should be kept to a minimum, preferably two (e.g. stucco and tile, brick and stone) or less. Using the same wall materials as adjacent or nearby buildings helps strengthen the character of the building.

4. Full size brick veneer is preferable to brick tile. Brick veneers should be mortared to give the appearance of structural brick. If used, brick tile applications should use wrap-around corners and bullnose pieces to minimize a veneer appearance.
5. Stone and stone veneers are appropriate as a basic building material or as special material for wall panels or sills in combination with other materials, such as brick or concrete.
6. Poured-in-place concrete options, in terms of formwork, pigments, and aggregates, should be explored to create rich surfaces. Accents such as ceramic tile or stone are recommended for decorative effect.
7. Concrete block is available in various sizes, surface textures, and colors. Decorative treatments, such as alternating courses of differing heights, should be used. Neither stack bond nor plain gray concrete block should be used.
8. Ceramic tile is recommended as an accent material.

CHAPTER 7.0

Design and Architectural Standards and Guidelines



Example of Appropriate Wall Surface Materials



Appropriate Prominent Window Opening

9. Stucco and/or painted stucco may be used in order to reduce maintenance and increase wear. Elastomeric type coatings should be used for painted surfaces. Textured stucco should not be used. All stucco surfaces should be smooth to prevent the collection of dirt and surface pollutants, and the deterioration of painted surfaces.

7.1.10 Windows

9. Windows are an important element of building composition and an indicator of overall building quality:
 - a) Window-to-wall proportion for the upper stories should have a window to wall area proportion (typically 30-50%) that is smaller than that of ground floor storefronts.
 - b) Window openings should generally be vertical or square in shape. If square, windows and/or windowpanes should be vertical in shape.

- c) Window inset glass should be inset a minimum of three inches from the exterior wall surface to add relief to the wall surface. This is especially important for stucco buildings.
- d) Shaped frames and sills should be used to enhance openings and add additional relief. They should be proportional to the glass area framed; e.g. a larger window should have thicker framing members.
- e) "True divided light" windows or sectional windows are recommended where a divided window design is desired. "Snap-in" grilles or mullions should not be used.
- f) Clear glazing is strongly recommended. Reflective glazing should not be used. If tinted glazing is used, the tint should be kept as light as possible; green, gray, and blue tint shades are recommended.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.1.11 Trellises, Canopies, Awnings and other Building-Mounted Accessories

- 2. Awnings are recommended. They should be made of a colorful fabric mounted over a metal structure that is framed and attractive in design. Fabric awnings are generally preferable to permanent canopies. Backlit awnings are “discourage.”
- 3. Trellis and Canopy materials, colors, and form should be derived from the building architecture.



Appropriate Use of Awnings

- 4. Placement of trellises, canopies and awnings should be above the display windows and below the storefront cornice or sign panel. They should not cover piers, pilasters, clerestory windows or other architectural features. An individual awning or canopy for each storefront or building bay complements the building more effectively than one continuous awning does.
- 5. Accessories such as colorful banners should be used to add variety to the street. Ornamental brackets and poles add further interest. Hanging flower or plant baskets suspended from ornamental brackets of metal or wood are recommended for storefronts.



Appropriate Banner Signage

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.1.12 Color

1. In general, unvarying earth tones should not be used throughout the project. Natural materials (such as stone) are encouraged on building walls to provide variation in texture and color. Building walls should be painted with contrast trim colors; for example, neutral or light walls with dark colors and saturated hues for accent and ornamental colors;
 - a) Secondary color can be used to give additional emphasis to architectural features such as building bases (like a wainscot), plasters, cornices, capitals, and bands.
 - b) Bright Colors should be used sparingly. Typical applications are limited to fabric awnings and banners. A restrained use of bright colors allows display windows and merchandise to catch the eye and stand out in the visual field.

7.1.13 Private Open Space

1. For Mixed Use Planning Areas 17 and 18, a minimum of 5000 square feet of recreation/open space shall be required per each planning area.

7.2 RESIDENTIAL DESIGN STANDARDS & GUIDELINES

7.2.1 Introduction

The following residential design guidelines provide the criteria necessary to achieve the desired neighborhood character for The Village Specific Plan.

7.2.2 General Urban Design Standards

These standards apply to all residential Planning Areas, except the Mixed Use Planning Areas, and all architectural styles. They are not meant to be restrictive, but rather to assist in the design, process, and construction of a higher quality design:

1. Overall, the standards are based on six essential components of good residential neighborhood design:
 - a) Architecture forward
 - b) De-emphasis of the garage
 - c) Variation of setbacks
 - d) Varied roof pitches
 - e) Eclectic variety of architectural styles
 - f) 360 degree architecture
2. It is specifically intended that all residential dwellings within The Village Specific Plan not be conventional "production housing" to which a few special historic details or high quality building materials are applied. Irrespective of the material used, it is critical that the massing, scale, and proportioning of the stylistic elements of each architectural style shall be authentic in character.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

3. Each architectural style shall be expressed authentically.
4. The following design elements shall be avoided: inappropriate building mass, scale or proportion, extreme interpretations of an architectural style; continuous flat blank walls without fenestration when exposed to public view; and harsh contrasts of materials and colors.
5. Sides and rear of residential and garage structures exposed to public view shall receive the same level of architectural enhancement as the fronts of the residential structure.

7.2.3 Residential Design Standards and Guidelines By Product Type

7.2.3.1 High Density Residential Design Standards and Guidelines

Building form within the High Density Residential Planning Areas consists of 3-story townhouses or “rowhouses” which possess many of the characteristics associated with single-family homes, including, but not limited to, large floor area, street or paseo facing front doors, and attached garages.

A. Neighborhood Compatibility

1. Front building facades should include porches, projecting eaves and overhangs, and other traditional architectural elements that provide residential scale and help break up building mass.

2. Massing and orientation of rowhouses should step back the third story to minimize visual impact.
3. Privacy of neighboring structures should be maintained, with windows on upper floors and balconies positioned so that they minimize the view into neighboring properties.
4. Sun and shade impacts on neighboring properties should be considered.
5. Where the side facades at the end of a rowhouse are oriented to a street, driveway, paseo, or neighboring property, massing and design quality should be consistent with other building facades.
6. End units facing a street should be designed to create a strong relationship with the street, with elements such as entries, wrap-around porches, and bays facing the street.



Illustrative Photo

B. Circulation Hierarchy

1. Rowhouses should be served by a distinct hierarchy of public streets,

CHAPTER 7.0

Design and Architectural Standards and Guidelines

internal streets, paseos, and driveways to provide circulation options for pedestrians, bicyclists and motorist and to clarify the relationship of dwellings to the streets.

2. Rowhouses should be configured to maximize the number of dwellings facing the public street.
3. Public Streets should allow as much on-street parking as feasible.
4. Internal streets should conform to the high quality standards and be designed to resemble public streets, with 5 feet wide sidewalks, parallel parking, and street trees.
5. Internal streets should provide loop circulation wherever possible rather than dead end cul-de-sacs.
6. Dead end driveways/alleys should be less than 150 feet long.
7. Alleys should have a width of at least 30 feet (Figure 5.6) and shall be lined with accent trees and planter strips to help soften the appearance of multiple garage doors.
8. Internal streets should connect to landmarks or amenity features, such as parks or community buildings, tot lots, and pocket parks.
9. Internal streets should incorporate special design features such as

special paving, neckdown intersections, and separated sidewalks with street trees.

10. All internal streets shall be designed to allow emergency vehicle access, per Fire Department requirements.

C. Paseos

1. Although internal streets are the preferred site design choice, front door access may be provided by an accessible pedestrian paseo network. Paseos should be designed as supplemental streets so that residents can clearly and comfortably access community amenities and the public street from visitor parking, and visitors can easily locate units.



Illustrative Photo

CHAPTER 7.0

Design and Architectural Standards and Guidelines

2. Paseo connections should be made wherever auto connections are infeasible due to project or site constraints.
3. Paseos should provide easy and direct access to building entries, common open space amenities and visitor parking areas.
4. Paseos should visually extend the street into an area for safe pedestrian use, with consistent street furnishings.
5. Paseos should be embellished with special paving and decorative lighting.
6. Rowhouses lining paseos shall provide windows along the building face to encourage comfortable and safe pedestrian use.

D. Private Open Space

1. Small private usable open spaces such as front yards, porches, patios, or decks shall be provided at a minimum of 100 square feet per unit.
2. Design of private open space should emphasize usability, with convenient access from the interior of units so that open space can be used as part of everyday living.



Illustrative Photo

3. Front yards should provide small extensions of the entry porch stoop or front living areas for semi private activity. The location, size, and access to yard space can vary depending on the lot layout of the residence.
4. Yards should include privacy hedges, low walls (no more than 3 feet in height), or low fences to provide separation from the sidewalk edge, and a sense of definition and enclosure.
5. Front yards should provide space for an entry, front stoop, or porch and landscaping between the public sidewalk and the rowhouse.
6. Yards should feature a balance of landscaping and paved area.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

E. Front Porches

1. Raised porches are encouraged. The first floor level should be raised approximately three to five steps above the grade of the sidewalk directly in front of the front entrance.
2. Porches may encroach into the front yard setback as permitted in this Specific Plan or in the City of Oxnard Zoning Code.
3. Porches should be oriented to sidewalk and building corners facing intersections.



Illustrative Photos

F. Building Design

1. Residences shall have front entrances on streets or paseos, and should avoid back-to-front facing relationships.
2. Buildings shall foster an appearance of a residential neighborhood. Individual units shall have a presence on the street and not be walled-off or oriented inward around a paseo or green space. Living areas with windows, decks and porches which overlook common areas, drives, and paseos are encouraged.
3. Entry features should include primarily single-story elements, or be incorporated into two or three story vertical elements to break up building mass along the street.



Illustrative Photo

4. Buildings facades and roof lines shall provide articulation to create identity for individual units. Long horizontal eaves and roof elements across the façade should be broken up with gables or other building projections and articulation.
5. Projecting eaves and roof gables shall be related to corresponding projections in building masses.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

Projections should extend beyond the main façade to increase building articulation (2 feet minimum is recommended).

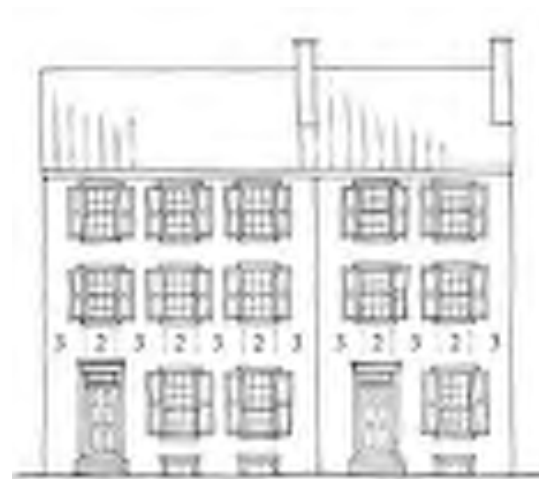
6. On corner lots, side yard facades shall maintain the architectural design quality consistent with the front façade.

G. *Building Materials*

1. Building materials shall be used in a consistent and harmonious manner throughout the residential neighborhoods.
2. Buildings shall emphasize high quality, durable materials that are harmonious with neighborhood style.
3. Changes in materials generally should not occur on the same plan, as this may result in an insubstantial or applied quality. Changes should correspond to variations in building mass.
4. Differentiation of dwellings is desired, but using dramatically different architectural styles for dwellings within the same neighborhood is generally discouraged.
5. The base of each residence should be clearly defined with a heavier material, such as brick or stone, or with a darker color than the rest of the building.

H. *Windows*

1. Windows shall be well organized on a building façade to create a rhythm or pattern.



Illustrative Rendering

2. Windows shall emphasize vertical massing of individual dwellings.
3. Windows shall have a hierarchy of size emphasizing the function of the living spaces and views while allowing for privacy of neighboring properties.
4. Windows shall be well detailed and consistent with the architectural design of the building.
5. Windows should be recessed from the exterior building wall and should be defined by well-designed trims on the exterior.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

I. Spaces Between Buildings

1. Spaces between buildings may be used as utility areas for air conditioning units, electric utilities and water meters.
2. Access to utilities shall be restricted either by landscaping or security gates.
3. Spaces between buildings shall be relatively unnoticeable from an angle and should be attractive from a pedestrian standpoint.
4. Spaces between buildings shall be landscaped with a combination of plants and/or hardscaped pathways.



Illustrative Photo

2. Garages must be located on the back of residences. Generally, garages should be recessed behind the back elevation wall plans. Where garage doors are flush with the upper level facades, the façade should feature upper level building projections, such as trellises to provide visual interest.
3. Landscape areas shall be included in driveway apron areas between adjacent units and garage doors.



Illustrative Photos

J. Garage Treatment

1. Garage doors will be the dominant feature along rowhouse alleys and should be selected to be consistent with the building architecture, with compatible details, materials, and colors.

CHAPTER 7.0

Design and Architectural Standards

7.2.3.3 Mixed Use Residential Design Guidelines

Please refer to Section 7.1 for all applicable Mixed Use Design Guidelines.

7.2.3.4 Very High Density Residential Design Guidelines

Building form within the Very High Residential Planning Area consists of 4-story condominium flats over two levels of subterranean parking or wrapped around a parking structure.



Illustrative Rendering

A. Neighborhood Compatibility

1. Front building facades should include porches, projecting eaves and overhangs, and other traditional architectural elements that provide residential scale and help break up building mass.
2. Massing and orientation of dwellings shall be stepped back on the third and fourth stories to minimize visual impact.
3. Privacy of neighboring structures should be maintained, with windows on upper floors and balconies positioned so that they minimize the view into neighboring properties.

4. Sun and shade impacts on neighboring properties shall be considered.
5. Setbacks, where provided, should include privacy hedges, low walls or fences (no more than 3 feet in height), to provide separation from the sidewalk edge, and a sense of definition and enclosure.
6. Setbacks, where provided, should feature a balance of landscaping and paved area.

B. Paseos

1. Paseo connections to neighboring streets should be made wherever auto connections are infeasible due to project or site constraints.
2. Paseos should provide easy and direct access to building entries, common open space amenities and visitor parking areas.
3. Paseos should visually extend the street into an area for safe pedestrian use, with consistent street furnishings.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

4. Paseos should be embellished with special paving and decorative lighting and landscaping.

C. Private Open Space

1. Design of private open space should emphasize usability, with convenient access from the interior of units so that open space can be used as part of everyday living.
2. The primary open space shall be the rear yard designed as a courtyard, or the central space between buildings. Courtyards can be located on the ground or on a podium. Side yards may also be formed to provide common use gardens or other similar public spaces.
3. The Very High Density Planning Area, shall include a minimum of 5000 square feet of recreation space in the form of pools, play areas, turf areas, and other appropriate recreational amenities.



Illustrative Photo

D. Building Design

1. Projecting eaves and roof gables should be related to corresponding projections in building masses. Projections should extend beyond

the main façade, to increase building articulation (2 feet minimum recommended).

2. Buildings façade treatments, roof lines, exterior walls, and windows shall be horizontally and vertically articulated to create identity for groups of dwelling units. Long horizontal eaves and roof elements across the façade should be broken up with gables, or other building projections and articulation.
3. Buildings shall foster an appearance of a village neighborhood. Individual units should have a presence on the street and not be walled-off or oriented inward. Living areas with windows, decks and porches which overlook common areas, drives, and paseos are encouraged.

E. Building Materials

1. Building materials shall be used in a consistent and harmonious manner throughout each building or group of buildings.
2. Buildings shall emphasize high quality, durable materials that are harmonious with the neighborhood architectural style.
3. Differentiation of dwellings is desired, but using dramatically different architectural styles for dwellings within the same building is generally discouraged.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

4. The base of each residential building should be clearly defined with a heavier material, such as brick or stone, or with a darker color than the rest of the building.

F. Windows

1. Windows shall be well organized on a building façade to create a rhythm or pattern.
2. Windows shall emphasize vertical massing of individual buildings.
3. Windows shall have a hierarchy of size emphasizing the function of the living spaces and views while allowing for privacy of neighboring properties.
4. Windows shall be well detailed and consistent with the architectural design of the building.
5. Windows shall be recessed from the exterior building wall and should be defined by well-designed trims on the exterior.

G. Spaces Between Buildings

1. Spaces between buildings may be used as utility areas for electric utilities and water meters.
2. Spaces between buildings shall be relatively unnoticeable from an angle and should be attractive from a pedestrian standpoint.

3. Spaces between buildings shall be landscaped with plants and/or hardscaped pathways.

H. Service Areas

1. Utilities, including utility cabinets, should be incorporated into the building within the property line, not located at corners, and not visible to the passerby.
2. Generators should be incorporated into the parking levels or rooftops of buildings, and should not detract from ground floor space that can be utilized for active uses.
3. Equipment on the tops of buildings should be enclosed and integrated into the building form and should not be visible from street and highway vantage points. Equipment includes mechanical, electrical, communications, emergency, air conditioning units, and other related typologies.
4. Trash service areas should be fully enclosed within designated structures. If the structure is on or adjacent to the street, these structures should include enhanced architectural treatments consistent with the style of the buildings. If the trash storage area is located within a parking garage, it shall be fully enclosed. The HOA or responsible agency shall be responsible for moving the trash bins to and from an approved street-side location.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.2.3.5 High Rise Residential Design Guidelines

Building forms within the High Rise Planning Areas consist of towers with above ground structure parking.



Illustrative Rendering

A. Building Orientation

1. Buildings shall be setback a minimum of 10 feet from the property line.
2. Orient structures such that urban open spaces receive adequate direct sun and filtered daylight and are protected from excessive wind, building glare and shade.
3. Two or more towers shall be offset to maintain more distant views for all including plants, water features
4. residents.
5. The primary building entry shall be oriented towards the public street. Parking and vehicle access should be located away from pedestrian entries, open space, and street intersections.

B. Building Design

1. Buildings should have a discernable treatment that differentiates the base, middle and top of all building facades.
2. Awnings and sunshade treatments are strongly encouraged for the south and west sides of the development in response to solar orientation.
3. The building entries should incorporate one or more of the following elements:
 - Increased ceiling height in lobby spaces
 - Distinctive doorways
 - Decorative lighting
 - Distinctive entry canopy
 - Projected or recessed entry bay
 - Building name and address integrated into the building façade
 - Artwork integrated into the façade or the sidewalk
 - A change in paving material, texture, or color within the property line
 - Distinctive landscaping, including plants, water features and seating
 - Ornamental glazing, railings, and balustrades
5. The tops of tall buildings shall be designed to provide visual interest to

CHAPTER 7.0

Design and Architectural Standards and Guidelines

the citywide skyline. While each building and complex of buildings should be designed for distinction, every building also should be designed within its context.

6. To ensure a cohesive and compatible night skyline, tower accent lighting should be modest and restrained. Bright color hues and neon outlines are prohibited, and white or warm color washes are preferred. Any signature lighting, including rooftop “lanterns” and other lighting effects, shall be designed with adjustable controls for subsequent testing and approval by the City of Oxnard Planning Department as part of design review.



Illustrative Photo of a Green Roof

7. Green roofs should be considered and are encouraged. Green roofs can provide private or common open space for residents of the development.
8. Penthouse space, mechanical equipment, stair and elevator overruns, heliports, vertical roof attachments, and decorative roof construction are permitted on the towers to achieve distinctive building tops, provided that the building top is designed as an

integral part of the architectural design.

9. Windows are to be as transparent as possible at the base of the building. The window tint may not exceed the extent where a face is not recognizable 20 feet inside the building. Glass above the ground floor will have a maximum reflectivity of 8% and stay in the cool color ranges (blue/green or blue/gray) if clear glass is not proposed.
10. The parking portion of a structure should be architecturally compatible with the rest of the building and streetscape.
11. The parking structure interior and roof deck lighting shall not be visible from street level viewpoints.

C. Building Materials

1. Exterior building materials shall be chosen with consideration of their glare causing potential, not only at the street level, but also from the view of other neighboring structures.
2. Value-added materials, such as stone, should be placed at the base of the building, especially at the first floor level. Materials should be selected that are suitable for a pedestrian urban environment. Impervious materials such as stone, metal or glass, should be used on the building exterior. Durability and maintenance

CHAPTER 7.0

Design and Architectural Standards and Guidelines

shall be prime considerations in material selection.

3. The choice of colors and cladding materials shall articulate the building's facades in intervals and provide a desirable scale in relation to the surrounding context.

C. Service Areas

1. Utilities, including utility cabinets, should be incorporated into the building within the property line, not located at corners, and not visible to the passerby.
2. Generators should be incorporated into the parking levels or rooftops of buildings, and should not detract from ground floor space that can be utilized for active uses.
3. Equipment on the tops of buildings should be enclosed and integrated into the building form and should not be visible from street and highway vantage points. Equipment includes mechanical, electrical, communications, emergency, and other related typologies.
4. Trash service areas should be fully enclosed within designated structures. If the structure is on or adjacent to the street, these structures should include enhanced architectural treatments consistent with the style of the buildings. If the trash storage area is located within a parking garage, it shall be fully enclosed. The HOA or responsible

agency shall be responsible for moving the trash bins to and from a street-side location approved by the City of Oxnard on the designated trash pick-up day(s).

D. Private Open Space

1. For the High-Rise Planning Area(s), a minimum of 6000 square feet of recreation space per planning area shall be provided for the residents.

7.2.3.5 Transit Center Design Guidelines

Building forms within the Transit Center Planning Area shall consist of 1 ½ to two-story in height structures and shall be consistent with the existing Wagon Wheel Motel's "Western" architectural theme (See Figures 3-3 and 7.1 for additional design standards and guidelines).

A. Building Orientation

1. Buildings shall be set back a minimum of 5-feet from the property line. The front of the building shall align with the street property line.
2. Buildings and all proposed pedestrian amenities shall be placed as close as possible to the adjacent railroad line to allow future use of the facility as a train/metrolink station.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

B. Circulation Hierarchy

1. Direct access to the Transit Center shall be provided from the street.
2. Separate bus and vehicle drop-off/pick-up areas shall be provided along the transit center's street frontage.

C. Site and Building Design

1. Overall building size shall be of a relatively small scale when compared to other buildings proposed as part of The Village Specific Plan
2. Building form shall be rectilinear in appearance and shall utilize simple framing methods.
3. The primary walls shall be expressed as single-plane expanses of wood or wood-like siding. The street-facing façade shall include decorative elements such as window molding, cornices, lighting, and signage.
4. The building design shall incorporate pedestrian friendly amenities such as porches, awnings, recessed entries, and planters along the sidewalks and public gathering areas.
5. Off-street parking shall be provided behind all proposed buildings in either surface parking lots, or parking structures. At least 50

parking spaces shall be designated for users of the transit center.

6. Where feasible, retail space shall be incorporated into the ground floor of the building to provide opportunity for a coffee shop/kiosk, newsstand, or similar uses.
7. Bicycle lockers and/or bicycle racks and bus shelter shall be provided as required by the City of Oxnard.
8. A kiosk or other similar feature shall be provided to display pertinent transit program information.
9. Where feasible, original signage from the Wagon Wheel motel/restaurant shall be incorporated into the building design. Where new signage is proposed, it shall be compatible with western period size, style, and lettering.
10. Roof shapes are to be compatible with the western style and the design of the existing Wagon Wheel Motel. Typically, roof forms are of a shed or single gable type with ridgeline running perpendicular to the street.

D. Building Materials

1. Only building materials that are consistent with the "Western" architectural theme shall be used on the building exterior. Preferred materials include, vertical wood (or woodlike) board and batten,

CHAPTER 7.0

Design and Architectural Standards and Guidelines

horizontal wood (or woodlike) siding, and brick masonry.

2. Window treatments shall emphasize vertical proportions in glass panels with multiple panes of glass rather than single large sheets.
3. Accent trim and framing of windows shall consist of wood or woodlike materials recalling the carpentry forms typically associated with Western themed architecture.
4. Where feasible, the original brick used to construct the Wagon Wheel Motel/Restaurant shall be salvaged and incorporated into the overall building design.
5. Roof materials shall be compatible with those typically used in Western Style. Preferred materials include composition shingle, slate or flat concrete tile, and corrugated or standing seam metal in color and finish compatible with western style.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.3 ARCHITECTURAL GUIDELINES

These guidelines govern the stylistic aspects of the design of buildings, appurtenances and site elements within the Specific Plan Area.

The following guidelines are not intended to create a rigid framework of rules but rather to describe the desired result and assist the Developer(s) in providing residential dwellings of appropriate style and character.

7.3.1 Architectural Authenticity

- Floor plans and massing solutions should be authentic to the architectural style.
- The massing of the structures as well as the architectural detail should be as authentic as possible.
- The overall intent of the design criteria is to avoid “stage front” architecture.
- Refrain from architectural gimmicks that sacrifice the integrity of the proposed architecture.
- Use progressive color palettes and define a selection of style-specific colors for homes and neighborhoods that can change over time to represent the uniqueness of a place and the values of its people.

- Authentic or natural-looking building materials are encouraged. These materials include wood, brick, smooth plaster, stone, tile, and slate as listed in the following standards for each style.

7.3.2 Architecture Forward

- The living areas of the home, not the garage, should be the predominant element in the street scene of a neighborhood. Floor plans that locate the garage behind residences and bring the living spaces of the home forward shall be provided.
- Wrap around architecture should be provided to enhance of side and rear elevations exposed to public view.
- Rear architectural treatments should be a well-conceived balance of hip and gable forms along a rear street scene exposed to public view.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.3.3 Architectural Styles

The Village Specific Plan is influenced by European architectural styles, architectural styles found throughout Oxnard and greater Ventura County, and other urban centers throughout the United States. The following examples of European architectural styles and their individual elements are provided to guide the Developer(s) during preparation of formal architectural elevations.

for understanding the principal concepts for this style.

Mediterranean

The Mediterranean style is derived from Spanish, Italian, Greek, and North African precedents. The Mediterranean style is noted for low-pitched hipped roofs typically covered by ceramic tiles and designed with broadly overhanging boxed eaves; arches above doors, first story windows or porches; entrances accented by small classical columns or pilasters; and less elaborate second story windows. Pedestrian features of the style may include the following elements:

- Recessed entry
- Full length first story windows facing the street
- Porches with massive square piers as porch supports
- Second story balconies

The design characteristics and elements as illustrated in the following images provide the essentials of massing, scale and proportion, and building materials



Illustrative Photo

CHAPTER 7.0

Design and Architectural Standards and Guidelines

Monterey

The Monterey style is a component of Mediterranean and the original Spanish-adobe construction methods. The style was popularized by the use of simple building forms, the introduction of wood framing and the addition of second stories. Roofs feature gables or hips with broad overhangs, often with exposed rafter tails. Shutters, balconies, verandas and porches are an integral part of the Monterey character. The design characteristics and elements as illustrated in the following images provide the essentials for massing, scale and proportion, and building materials for understanding this particular style.



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CHAPTER 7.0

Design and Architectural Standards and Guidelines

European Cottage

The European Cottage is a style that evolved out of the Medieval Tudor and Norman architecture. Roof pitches for these homes are steeper than traditional homes, and are comprised of gables, hips and half-hip roof forms. The primary material is stucco with heavy use of stone and brick bases, veneers and tower elements. Some of the most recognizable features for this style are decorative half timbering, tall narrow windows, the stucco accents in prominent gable roof forms and large chimneys with decorative tops. The design characteristics and elements as illustrated in the following images provide the essentials for massing, scale and proportion, building materials, and details for understanding this style.



Prominent Gable Roof Forms



Decorative Half-Timbering



Tall Narrow Windows



Decorative Chimneys



Illustrative Photo

CHAPTER 7.0

Design and Architectural Standards and Guidelines

Spanish

Spanish Revival style is a culmination of Spanish styles brought to a sophisticated level of interpretation in the early 1890s. Borrowing elements of detailing from Moorish, Byzantine and Renaissance architecture in Spain, Spanish revival includes massing with the use of ornate classical elements and details at entries, arcades, windows and balconies. Although the building mass remained simple, the style exemplified sophisticated fine classical detailing. The design characteristics and elements as illustrated in the following images provide the essentials for massing, scale and proportion, building materials, and details for understanding the primary concepts for this style.

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CHAPTER 7.0

Design and Architectural Standards and Guidelines

California Craftsman

The Arts and Crafts movement, which spawned the Craftsman architectural style, was in part brought about by the expansion of the Industrial Revolution in England during the 1830's. Unhappy with the idea of "division of labor" which was the foundation of modern factory work, notable English designers began focusing on the value of traditional craftsmanship, beauty and artistry. After the end of the Arts and Crafts movement in England (around 1896), notable architects, such as Frank Lloyd Wright, and furniture maker Gustav Stickley truly defined the American Arts and Crafts movement in the early 20th Century. Buildings are composed of horizontal, single and multi-story volumes. Walls typically include horizontally placed wood siding, shingles, shingles, or board and batten, with a foundation base and piers in river stone. Common architectural design features are:

- low pitched roof lines, gabled or hip roofs
- deeply overhanging eaves
- exposed rafters
- front porch beneath expansion of main roof
- tapered, square columns supporting roof
- 4-over-1 or 6-over-1 double hung windows
- handcrafted stone or woodwork
- mixed materials throughout structure



Illustrative Photos

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.4 SUSTAINABILITY DESIGN GUIDELINES AND STANDARDS

7.4.1 Introduction

The intent of this section is to provide requirements as well as options for builders and designers on building materials and systems that tend to have less environment impact and improve the quality of life for occupants and the community. These standards and guidelines apply to all Planning Areas proposed as part of The Village Specific Plan.

7.4.2 Energy

- Buildings shall be oriented and include the use overhangs, shutters, louvers, and shade trees to minimize solar heat gain.
- Where feasible, buildings and blocks should allow for the passage of cooling breezes.
- Buildings should minimize the amount of conditioned space by using covered outdoor circulation, porches, balconies, and arcades.
- Buildings should utilize tight, well-insulated walls systems and high performance glazing.
- Solar plumbing stub outs and electrical pre-wiring shall be provided in each High Density building type and made available to the buyer as an option.

- Multi-Story buildings should consider the use of green roofs to minimize solar heat gain, restore ecological and aesthetic value to form of urban open space, and reduce roof runoff.

7.4.3 Indoor Environmental Quality

- Buildings should be organized to provide abundant natural light and a constant supply of outdoor air.
- Mechanical systems shall be designed to accommodate operable windows.

7.4.4 Resource Conservation

- To the extent feasible, the developer should consider using building materials that are made using a “closed loop” manufacturing process (e.g. products that are made using waste products or renewable resources as the raw materials for new products).

7.4.5 Water Quality

- To the extent feasible, storm water treatment infrastructure should be designed to promote site infiltration and reduce contaminants through biological filtration.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- To the extent feasible, opportunities for street side infiltration shall be incorporated into the landscaped median/parkway designs along the proposed streets.
- Permeable pavement should be used wherever feasible to allow runoff to infiltrate unto the underlying reservoir or base soil. Acceptable types of pervious pavement include, but are not limited to:
 - Porous asphalt that is comprised almost entirely of stone aggregate and asphalt binder with very little fine aggregate;
 - Pervious concrete that has a permeability rate of 12"/hour and has the appearance of exposed aggregate concrete.
 - Unit pavers/bricks/stone that provide a durable and attractive surface, have been spaced to expose a permeable joint and placed on a permeable base;
 - Crushed aggregate that provides a wide variety of aggregate types, and which must be bounded by a rigid edge;
 - Turf block;
 - Cobbles which are suited for low traffic areas and require a rigid edge.

7.5 LANDSCAPING DESIGN STANDARDS & GUIDELINES

The following sub-sections outline the conceptual as well as detailed design standards and guidelines for the landscape components within The Village Specific Plan. The design standards and guidelines described in this section also constitute regulatory standards for future landscape design within The Village Specific Plan area. The more detailed landscape design and construction documents which are prepared subsequent to adoption of this Specific Plan shall conform to and be consistent with the conceptual landscaping designs provided in Chapter 5.0 of this Specific Plan and the following standards and guidelines.

7.5.1 General Site Landscape Standards

- Design landscape and open space areas shall be an integral part of the overall site plan design, with a style and amenity level consistent with the surrounding environment.
- Trees shall be used to define and enclose exterior spaces, and to provide physical protection from the sun and wind.
- The design of landscaped open space areas shall enhance the building design, create meaningful public viewsheds, and provide buffers and transitions between adjacent uses.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- Trees, shrubs and vines shall be used to conceal walls, building elevations and parking facilities.
- Arbors, trellises, walls, gates and other structural elements shall be used to reinforce the architecture of adjacent buildings. Landscaped structures should be compatible and consistent with the architectural treatment of the adjacent buildings.



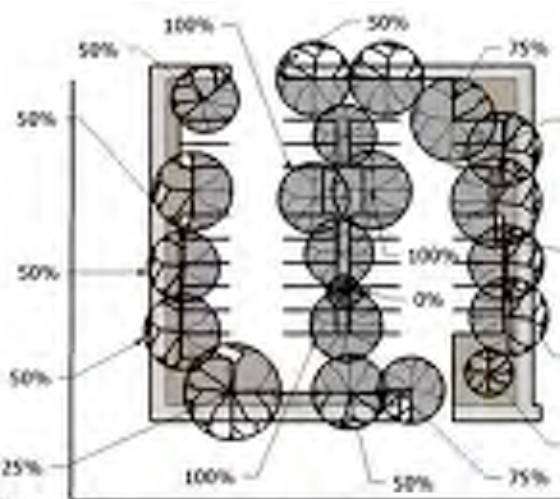
Illustrative Photo

- Undeveloped or otherwise vacant land intended for future development shall be screened with plants but provide unobstructed visual access for vehicles and pedestrians.
- Plant materials shall not interfere with security lighting, or restrict access to emergency equipment such as fire hydrants or fire alarm boxes.
- Any structures surrounding the mailboxes should match the style

of the homes/business where they are located

7.5.2 Surface Parking Area Landscaping Standards

- Each tree utilized within parking areas shall be located in such a way that on average, at least 30% of parking surface is shaded with tree canopy within five years of installation. Please refer to the diagram below for an example illustration.



Notes:
1. This diagram is intended to reflect the manner in which shade is credited under various conditions.
2. Trees may receive 25%, 50%, 75% or 100% as shown.
3. Shade overlap is not counted twice.

- Surface parking lots shall be landscaped and maintained with a combination of trees, shrubs, and groundcovers.

CHAPTER 7.0

Design and Architectural Standards and Guidelines



Illustrative Photo

- Surface parking lots shall utilize “Orchard Style” tree planting for shade and screening purposes. Island finger planters shall include at least 2 trees (one tree in each end of the island) and shall be at least 9 feet in width and 18 feet in length. Compact parking is not permitted.
- Trees shall be distributed throughout the surface parking areas.
- In parking lots with more than 50 spaces, trees shall be massed in large planters to reduce the apparent size of the parking lot.
- Ensure through tree choice and maintenance that the lowest tree branches are more than eight feet above the finish grade at the base of the tree to prevent damage from and to automobiles, pedestrians, and bicyclists.

7.5.3 Service/Loading Area and Refuse Enclosure Landscaping Standards

- Screen loading and service areas (including storage, special equipment maintenance, and loading areas) with landscaping and walls to conceal these areas from adjacent buildings, streets, sidewalks and driveways.
- Screen refuse collection and storage areas in an accessible interior, side or rear yard to the satisfaction of the City Engineer.



Illustrative Photo

- Position service and loading areas to prevent disruption of the traffic flow by service vehicles to and from the site.
- Locate loading areas, including space for truck maneuvers, completely on-site. Off-site vehicle loading is prohibited.
- Screen utility equipment and communication devices (antennae,

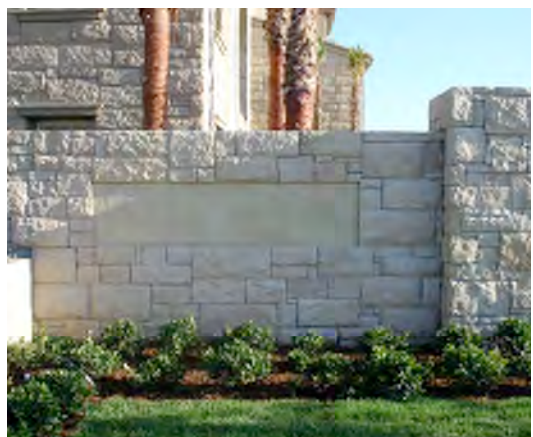
CHAPTER 7.0

Design and Architectural Standards and Guidelines

satellite dishes, etc.) so that the site appears free of all such devices.

- Install underground all utility connections equipment for electrical and communication.
- Construct and maintain service, storage, and maintenance areas according to the following criteria:
 - Store materials, supplies and equipment inside an enclosed building to prevent visibility from the neighboring property and streets.
 - Screen all storage areas with architectural solid screening walls (minimum six feet, maximum eight feet high).
 - Construct architectural screening walls (minimum of six feet, maximum eight feet high) with materials and finishes compatible with and complementary to the building design.
 - The intrusion of service, storage, maintenance, parking and loading areas into required landscape setbacks is prohibited.

- Use walls and fences for security, visual screening and aesthetic purposes. The edges located along the Ventura Freeway and Union Pacific Railroad shall have solid masonry walls which prevent human access and mitigate noise.
- Ensure that walls and fences are consistent with the architecture of the adjacent building in terms of material, color and form.



Illustrative Photo

- Avoid walls which feature long, unarticulated surfaces. Use pilasters, height variation, setback variations, landscaping, and surface texture or material to vary the wall surface.
- Walls and fences shall be of the following materials: masonry, with or without stucco finishes, stone or brick veneer, wrought iron and/or painted steel tubing. Wood, chain link, or razor wire

7.5.4 Wall and Fence Landscape Standards

CHAPTER 7.0

Design and Architectural Standards and Guidelines

on top of fencing will not be permitted.

- Prohibit walls from encroaching into areas needed for vehicle safe sight-distance.
- Ensure that walls intended to screen storage or refuse areas, outdoor mechanical equipment, or which separates different uses at a property line be of masonry construction and not be less than 6 feet high, and not more than eight feet in height.
- Walls and fences greater than thirty-six inches in height shall be prohibited within the required setbacks adjacent to public streets.
- Where appropriate, walls should be covered with living vines in order to discourage graffiti and enhance the appearance of the fence and/or wall.

7.5.5 Landscape Paving Design Standards

These standards for pavements apply to all paved areas on private property intended for use by pedestrians, including enhanced paving areas to be used by both pedestrians and vehicles. Pedestrian pavements may include, but are not limited to sidewalks, paths, walkways, courtyards and plazas.

- Paved surfaces intended for pedestrian and/or bicycle use shall have the following qualities:

- A surface texture rough enough to prevent slipping, but smooth enough to prevent stumbling;
 - Maintenance-free and/or low maintenance;
 - Stain-resistant;
 - Fade resistant; and
 - Non-reflective
- The following pedestrian paving materials meet these criteria :
 - Colored concrete: broom finished, salt finished, heavy sandblasted; and
 - Stamped and saw-cut concrete and tile, provided the pavers do not have joints or score lines that catch high heels, or cause tripping.
 - Other pedestrian paving surfaces which do not meet these criteria may be used, provided that the limitations of the material have been considered:
 - Decomposed granite (not suitable for use where disabled access should be provided);
 - Loose gravel (not suitable for use where disabled access should be provided, or where heavy pedestrian traffic can be expected);

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- White or very light colored paving (not suitable where glare from surface will affect pedestrian safety);
- Asphalt (not suitable where the asphalt is likely to become soft on hot days); and
- Wood boardwalk paving (not suitable where heels might catch in the cracks between the boards).

7.5.6 Landscape Plant Palette and Planting Design Standards

- Provide simple, bold and easy-to-maintain landscape planting designs which incorporate many drought-tolerant non-toxic plant materials.
- Landscape elements on development parcels visible from the public right-of-way should blend with and appear to be an extension of the public right-of-way landscaping.
- All street trees and median trees are to be a minimum of 36 inch box size. All other trees are to be a minimum of 24" box size, except where indigenous tree plantings occur. All shrubs shall be 5 gallon size. Ground cover shall be planted at 12 inches on center maximum spacing; one-gallon container ground cover shall be planted at 3 feet on center.

- Where necessary, trees shall be planted in a minimum 5' x 5' tree well cut-out with a minimum 7' radius of structural soil from the center of the tree.
- Provide a mix of plant material sizes in informal planting areas. The following is a typical mix of plant sizes for an informal planting.

Trees:

- 60%, 24-inch box
- 25%, 15-gallon
- 15%, 5-gallon

Shrubs:

- 70%, 5-gallon
- 30%, 1-gallon

Ground Cover:

- 100% coverage, within 1 year

- Provide plant palettes that include both long and short-lived plant materials, as identified in Table 5.1. Long-lived materials include trees and most shrubs. Short-lived materials include perennials, annuals and some shrubs.
- Select plant materials suited to the soil and climatic conditions of the site, as identified in Table 5.1.
- Minimize the use of water through the selection of plants that are drought-tolerant when

CHAPTER 7.0

Design and Architectural Standards and Guidelines

they are mature, as identified in Table 5.1.

- Space trees and shrubs with consideration for their ultimate size.
- Ensure that all ground cover material is healthy, densely foliated, and is comprised of well-rooted cuttings or container plants. Ground cover planted from flats should be planted no more than eighteen inches on-center. One-gallon container ground cover should be planted no more than four feet on-center.
- All Landscape improvements shall meet the requirements contained in the City of Oxnard Landscape Standards, prepared by the Parks and Recreation Department, and be consistent with the design character and intent of this section and Chapter 5.0 of this specific plan.
- Refer to The Village Landscape Plant Materials List (Table 5.1) for specific recommended plant types within The Village community. Any landscape issues not addressed in the Specific Plan will be referred back to the City of Oxnard Landscape Standards for a final determination.

7.5.7 Irrigation Design Standards

The landscape irrigation concept for the community will be designed to provide the most efficient means to distribute irrigation water and provide the village community with the latest technology for community-wide water conservation. The following standards apply:

- Irrigate all permanently landscaped or required landscape areas with a permanent underground irrigation system operated by automatic irrigation controllers.
- In areas where native or drought-tolerant plant materials are utilized, a drip irrigation system shall be installed to maximize the most efficient watering system and utilized the water in a responsible manner.
- Irrigate turf areas with low flow spray heads having a minimum six inch pop-up body.
- Use triangular spacing of spray heads in turf areas whenever feasible.
- Irrigate shrub areas with low flow spray heads. When appropriate, drip irrigation should be provided in shrub areas.
- Irrigate individual shrubs with a pressure compensation bubbler with low flow characteristics.
- Irrigate at night and early morning to reduce evaporation due to sun and wind.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- Apply irrigation water in short interval applications to reduce runoff to paved areas.
- Ensure that irrigation water is not applied to walks, driveways, walls, fences and building faces. Irrigation runoff water shall not flow on to other properties or on the public right-of-way. Provide methods of retaining irrigation runoff water on site.
- Through the application of Best Management Storm water Practices, graywater will be recycled and reused for irrigation water where appropriate.
- Provide approved backflow preventers on all systems.
- Screen all irrigation equipment from public view, including back-flow preventers and controller boxes.
- Install automatic valves in valve boxes and use pop-up spray heads whenever feasible.
- All irrigation installation shall meet the requirements contained in the City of Oxnard Landscape Standards prepared by the Parks and Recreations Department.
- Maintain all landscape materials, including trees, shrubs, ground covers and vines in a healthy condition at all times.
- Remove damaged, dead, or diseased plant material promptly and replace it with plants that match the original design intent in terms of size, location and variety.
- Remove overgrown, oversized, or hazardous plant materials when they cannot be pruned to a safe condition and threaten public health, safety or welfare.
- Prepare and show maintenance specifications on landscape plans describing the irrigation, pruning, weeding, fertilizer application and other pertinent maintenance criteria for all landscaped areas.
- All landscape maintenance shall meet the requirements contained in the City of Oxnard Landscape Standards prepared by the Parks and Recreation Department.

7.5.8 Landscape Maintenance Standards

7.5.9 Street Furnishings and Hardware

A consistent and well-designed selection of street furnishings will be utilized within The Village Specific Plan. The intent is to provide a unified, functional and visually appealing array of elements including pedestrian and vehicular street lights, transit waiting

CHAPTER 7.0

Design and Architectural Standards and Guidelines

canopies, kiosks, tree gates and guards, benches and trash receptacles.

In general, these elements will be readily available through an “off-the-shelf” catalogue. Components may be modified to create a unique and differentiated appearance for each public space within The Village Specific Plan. All of these elements will have a consistent style and will enhance the landscape theme of the community. Some custom features may include:

- Special or unique color employed throughout the community.
- Specific materials that reinforce a continuity or visually cohesive quality.
- Incorporation of The Village logo.

Refer to Table 7.1 for further examples of appropriate streetscape furnishings and hardware.

Table 7.1 - Examples of Appropriate Street Furnishings and Hardware



Benches: shall be provided within public spaces. They shall be decorative and can be comprised of either iron, wood, or durable woodlike material.



Street Furniture: shall be provided primarily within public spaces designated for outdoor dining. The wide sidewalks proposed along Village Parkway provide ample opportunity for use of street furniture.



Trash Receptacles: shall be provided within public spaces. Receptacles shall be decorative and consistent with the surrounding architectural theme. They can be comprised of wrought iron or other durable material.



Pedestrian Scale Lighting: shall be used throughout The Village Specific Plan area, particularly along Village Parkway. Fixtures shall be decorative and compatible with the architectural theme. Light levels shall be kept at the minimum in keeping with minimum pedestrian safety standards and the desired ambiance.

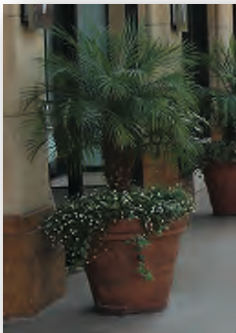


Street Clock: If utilized, the fixture shall be decorative and compatible with architectural theme.

Table 7.1 (Cont.) - Examples of Appropriate Street Furnishings and Hardware



Seat Wall: Where appropriate, seat walls can be integrated into public gathering spaces and can also be utilized as fountains or raised planters.



Potted Plants: Where appropriate, potted plants should be used to further enhance storefronts or public gathering spaces. Pots shall be consistent with architectural theme.



Bollards: Where appropriate, bollards should be used to separate pedestrian walkways and public gathering spaces from vehicle travel areas. Bollards shall be consistent in style with the architectural theme.



Raised Tree Wells: Where appropriate, raised tree wells should be used to highlight particular accent or specimen trees within public gathering spaces. These tree well can also double as seat walls and incorporate elements of public art.



Street Vendors: Where appropriate, street vendors should be encouraged as a means to further activate the public realm. The structures shall be temporary and shall be consistent in style with the architectural theme.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.5.10 Public and Community Art

Public art will be developed within The Village Specific Plan. Timeless, environmental, or functional pieces such as fountains, benches, street furniture and earthworks will be encourage, while monumental, abstract art elements will generally be discourage and considered in-appropriate. The public are element of The Village’s Landscape Master Plan is to be further developed as specific development plans are refined and implemented.

The conceptual park designs illustrated in Figures 5.8 through 5.12 identify potential locations for public art which have significant visual prominence and focus. Other locations may emerge as detailed development plans evolve.



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CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.6 SIGNAGE STANDARDS

7.6.1 Application

The regulations of this Section shall govern the design and maintenance of all signage within The Village Specific Plan Area, including on public rights-of-way, in parks, and within the each development component. These regulations apply to all signage designed, installed and maintained by the Builder/Developer and businesses in The Village.

The regulations are intended to result in functional, attractive signage that incorporates a uniformly high level of design, graphics, continuity, consistency and maintenance throughout the Specific Plan Area. More detailed guidelines will be included in a Signage Master Plan that will be prepared by the Developer(s) following adoption of this Specific Plan.

- In addition to conformance of signage to the requirements of this sub-section, all signage within The Village Specific Plan Area shall also conform to the specific requirements, spirit and intent of the Oxnard City Code provisions regulating signage (Chapter 16, Article IX Advertising Signs), unless otherwise stated. If there are any inconsistencies between the Oxnard code and this sub-section, the regulations of the Specific Plan shall take precedence.

- The Developer(s) must prepare and submit a Signage Master Plan as part of the submission package for review and approval by the City of Oxnard. This plan shall be based on the regulations of this Section and subsequent documents.

All permanent and temporary signage within The Village Specific Plan boundaries are regulated by this Specific Plan and subsequent Signage Master Plan. The types of signage addressed by these regulations include, but are not limited to the following:

- *Project Identity:* Signs which identify groups of commercial or residential development. Design for this type of sign will be similar to pole-mounted and monument signs for private commercial and residential development.
- *On-Building:* Signs upon buildings which contain the names and/or addresses of individual facilities (such as a building street number, street sign, or business name) or of a specific development (such as the name of a group of residences).
- *Public Transportation Signage:* Signs on freestanding benches and shelter signs.
- *Ground-Mounted Traffic Control:* Stop signs, etc.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- *Directional:* Signs on pedestrian and vehicular pathways or within parking facilities that assists the vehicular driver or pedestrian in reaching particular facilities or areas with The Village.
- *Wayfinding:* Signage which assist pedestrians and drivers in orienting themselves to The Village, its relationship to surrounding areas, and how to navigate The Village street and pedestrian system.
- *Temporary and Advertising Signs:* Identification of construction, advertising, for sale and for lease signs.

7.6.2 General Signage Standards

7.6.2.1 Design Quality

- All signage will achieve the highest level of design quality and be consistent with the quality required in all other sections of this Specific Plan.
- All signage will be consistent with the architectural and landscape character of the specific parcel development and with the design intent of The Village Specific Plan as a whole. This consistency shall be determined in terms of materials, scale, size, and placement on buildings or in the landscaping; integration with the building's architectural design, texture,

color, relation to other signage in the immediate vicinity and in the public spaces; and type of illumination. The scale and proportion of graphics shall be consonance with the design of buildings, individual stores, landscape and site design.



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- Bold or inharmonious colors or color combination shall not be used.

7.6.2.2 Conformance with Public Agency Requirements

- Signage setbacks will be required at all intersection and curb cuts in conformance with the California Department of Transportation Highway Design Manual, and as acceptable to the City of Oxnard Public Works and Planning

CHAPTER 7.0

Design and Architectural Standards and Guidelines

Departments. Driveways will be treated as private roads for sight distance purposed.

- All traffic control signs in public streets shall conform to the applicable California Department of Transportation and City of Oxnard Standards in construction, erection and placement.

7.6.2.2 Prohibited Sign Types

- Internally-illuminated awnings
- Plastic-faced monument signs
- Conventional plastic-faced box or cabinet signs
- Formed plastic or injection molded plastic signs
- Channel letters with light-transmitting acrylic faces, with the exception of Commercial Top of Building Wall Identity Signs, as discussed below under Section 7.5.3.4.
- Luminous vacuum-formed letters
- Animated or moving signs
- Cloth, paper, cardboard or foam signs or decals
- Moving neon signs
- Blinking, flashing, animated or moving signs

- Advertising displayed on vehicles to attract attention to a specific business location or sale
- Signs located or sized to impede views of neighboring signs, businesses, traffic or pedestrians
- Outdoor advertising signs, as defined in Section 16.596 of the Oxnard Municipal Code.
- Can or box-type signs

7.6.2.3 Signage Lighting and Design Elements

- All lit signs shall be front lit or halo lit and not internally illuminated, with the exception of Commercial Top of Building Wall Identity Signs, as discussed below under Section 7.5.3.4.
- No sign shall have visible moving parts or simulated movement by means of fluttering, rotating or reflecting devices. Moving signs (signs that either physically move or have fixed graphic and text elements that appear to move) may be permitted only by approval of a request for deviation from the design standards.
- No sign shall have flashing, blinking, or moving lights, or any other illuminating devices that has changing light intensity, brightness, or color, except for

CHAPTER 7.0

Design and Architectural Standards and Guidelines

parts designed to give public service information such as time, date, temperature, or similar information.

7.6.2.4 Signage Materials and Fabrication

- All signs shall be made of materials compatible with exterior building colors, materials, and finishes, and be of high-quality fabrication.
- Metal signs may be made of treated aluminum, brass, bronze, copper or stainless steel.
- Signs are to be free of all labels and fabricator's advertising, except for those required by code.
- All electrical service to sign lights shall be fully concealed. No sign shall have exposed wiring, raceways, ballast, conduit, or transformers.

7.6.2.5 Signage Content

- Logos shall be incorporated into signage in a three-dimensional manner.
- Logos or trademark displays may be used on signs if they are found to be unobtrusive and consistent with the aesthetic goals of The Village.

- Color and contrast of signs shall be chosen to insure proper readability and graphic consistency.

7.6.3 Standards for Specific Sign Types

7.6.3.1 Public Area Signs

- Sign types allowed in public areas and private neighborhood recreation areas are limited to wayfinding, directional and identification signage. Examples of such signage include gateway and project entry signage, neighborhood designation signage, park and other public realm element designations, and project maps and directories. Advertising signage, with the exception of the Village Logo, is not permitted.
- The Village Logo and typestyle will be used consistently on all public area signs. The Village Logo and typestyle will be defined in the Signage Master Plan. A secondary type style in upper and lower case will also be permitted to augment the primary type style, particularly on smaller signs such as legal notices. Other letter types may be permitted subject to design review approval.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.6.3.2 *Freestanding Monument Signs*

- Monument signs are encourage. They shall have low, horizontal proportions to create a feeling of permanency.
- Monument signs may not exceed six feet in height, but may be raised to reach eye-level by providing earth berms below. Berming will be subject to design review.
- The design of freestanding monument signs; including color, materials, and fonts; shall be integrated with landscaping and surrounding buildings, walls, other construction and landscaping.
- Monument signs shall have distinct base and cap elements. Monument signs shall be set back at least five feet from the frontage or regulating line and located in a landscaped area.

7.6.3.3 *Freeway-Oriented Pole Signs*

- Tall freeway-oriented pole signs are not permitted.

7.6.3.4 *Commercial Top of Building Wall Identity Signs*

- Top-of-building identity signs shall be limited to identifying a single tenant or building. Signs may be located on top floor fascia

or spandrel area on a maximum of two elevations. These signs may be internally illuminated or non-illuminated.

- The size and location of each sign shall be complementary and proportional to each building and group of buildings.
- In no case shall any sign exceed the sizes established by the following table:

Table 7-2
Commercial Top of Building Wall
Identity Sign Parameters

<i>Building Height in Stories</i>	<i>Maximum Letter Height</i>	<i>Maximum Square Feet</i>
1	2'-0"	80
2	2'6"	120
3	2'8"	160
4-6	3'-0"	200

Square footage designated by Table 7-2 is determined by a continuous rectangle surrounding the sign lettering.

- Creative backgrounds, light features, etc. may exceed the 24 inch letter boundary, subject to design review.
- Signs may be constructed of metal and/or acrylic. Other materials are subject to approval.
- On-building signs will have either internally illuminated or non-illuminated lettering and graphics. Illuminated commercial, top of building wall, and/or

CHAPTER 7.0

Design and Architectural Standards and Guidelines

identity Signs shall be reverse backlit, channel lit, or indirect lit. On-building signs will have individual letters mounted directly onto the building façade.

- Recommended sign types use internally illuminated letters with “halo effect,” or non-illuminated letters.



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7.6.3.5 Commercial Blade and Projecting Signs

- The total number of blade signs shall not exceed one per tenant; except that a tenant with frontage on more than one street may not exceed two blade signs, each sign to be located on a separate frontage.
- A sign panel may not exceed five square feet; the total sign area, including bracket, may not

exceed ten square feet. A blade sign may not project more than 3 feet from the building façade.

- All blade, hanging or otherwise projecting signage should be placed out of reach.
- Blade signs may be constructed with a metal bracket and a wood or metal sign panel; other materials are subject to approval.
- Indirect illumination is permissible.



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7.6.3.6 Commercial Tenant Identification Signs

- Tenant signs shall be limited to the identification of building tenants. In no event shall there be more ground floor on-building signs than tenants, and an individual tenant may not have more than two signs per building elevation. These signs shall be individual lettered and may be not be internally illuminated.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- A wall sign for tenant identification purposes may not exceed 22 square feet. Letters may not exceed 24 inches in height.
- Construction may be of metal, acrylic, or other high quality material permitted by the Uniform Building Code.
- The size and location of each ground floor on-building sign will be appropriately scaled to the building area and height. They shall be designed to be complementary and proportional to each individual building.
- Illuminated wall signs are permissible only when illumination is reverse backlit, channel lit, or indirect.

7.6.3.7 Commercial Storefront Signs

- All signs should be in proportion to the storefront size and scale of the individual business façade.

If signage and graphics are placed within a window area, they should be designed to augment the display of merchandise and appropriately scaled so as not to dominate the window area.

7.6.3.8 Residential Signs

- The only signs permitted for buildings within the High

Density, High Rise, and Very High Density Planning Areas are street address numerals, which may be up to 8 inches in height and shall be made of ceramic tile or bronze attached to a wall.

- Live/work residential dwellings are permitted either a blade sign or window area signage for identification pursuant to the previous standards.
- Signs pertaining to the sale or lease of a dwelling unit, trespassing signs, signs identifying the apartment or condominium project, and rental information signs are permitted.
- Courtyard Wall Signs are permitted to identify the apartment or condominium project and address. Lettering may be applied to Courtyard Walls (walls parallel to the Frontage Line and matching the building walls), and shall consist of individually mounted metal letters, not to exceed sixteen inches in height, either front lit or halo lit. Courtyard walls intended to receive signage may encroach to within five feet of the property frontage.
- Residential directional signs are permitted within individual residential neighborhoods to control and direct the circulation of vehicles and pedestrians. Directional signs shall only

CHAPTER 7.0

Design and Architectural Standards and Guidelines

provide functional directions to important locations throughout the Specific Plan.

- Monument signage is permitted to identify individual neighborhoods within the High Density Residential Planning Areas, subject to the standards established in Section 7.6.3.2.
- Language shall be as succinct as needed to convey the message.

7.6.3.9 Traffic Control Signs

- All street signage shall conform to City of Oxnard Standards.

7.6.3.10 Temporary Signage

- Future facility signs are temporary signs which display information regarding future projects. The Builder/Developer may display a temporary free-standing sign adjacent to Oxnard Boulevard, Ventura Road or Main Street (two signs maximum) which presents information pertinent to a site and its stage of development.
- A temporary free-standing sign may be erected on a site any time after subdivision and completion of tract improvements. Information may be added or the sign can be replaced by another during the design, construction and leasing periods. Signs may include such information as the

development's name, special features, architect and other design consultants, developer, month and year of completion, beginning of construction, advertisement to recruit employees, to identify the leasing agent, or similar information. Each revision or sign replacement must conform to these standards. A sign that is to be replaced by another shall be removed before the new sign is installed.

- Temporary signs may be post and panel signs or free-standing monument signs. Such signs shall be submitted for design review and must receive approval prior to installation.
- No temporary promotional signs will be permitted within public street rights-of way.
- Temporary signs shall be no more than five feet in height or four feet in width.
- Temporary signs shall be designed to last the length of its intended use without significant fading, warping, peeling, blistering, cracking, or rotting. The City reserves the right without liability to cause removal of any sign deemed to be in deterioration or damaged condition, or any sign that represents a public hazard.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- Temporary signs shall remain in place no more than twelve months. Removal shall occur after 95% occupancy or lease-out or within 6 months of occupancy approval, whichever is sooner. This period may be extended upon written approval of the City of Oxnard.
- No temporary sign shall pose a hazard to the safe movement of traffic and shall not block the visibility of permanent signs on adjacent lots.

7.6.4 Sign Maintenance

- All signs within The Village shall be maintained in an as-new and fully-functional condition. Signs shall meet all relevant maintenance standards of the City of Oxnard and of this Specific Plan at all times. The entity responsible for maintenance of the commercial spaces shall make periodic inspections of all signs on site. Any deficiencies shall be immediately corrected by the person(s) or business(es) responsible for the maintenance of said sign or signs.

7.6.5 Signage Review and Approval

- A signage plan must be prepared in accordance with these standards, or the Signage Master Plan when completed, submitted to the Master Developer for pre-approval, and submitted to the

City for review by the City as part of the complete submission package.

- Subsequent permanent and/or temporary signage must also be pre-approved by the Master Developer, and then submitted to the City for review and approval by the City.

7.6.6 Conformance

- No person shall erect, construct, enlarge, alter, move, improve, remove, convert or equip any sign or sign structure, or cause or permit the same to be done contrary to or in violation of the provisions of the Signage Master Plan.
- Conformance will be strictly enforced. All nonconforming or unapproved signs must be brought into conformance at the expense of the persons responsible for the installation of the sign or sign structure.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

7.7 EXTERIOR LIGHTING STANDARDS

7.7.1 Introduction

The objectives for exterior lighting are as follows:

- To contribute to the safe and efficient use of all public and private areas in The Village.
- To increase the perception and reality of personal and property security.
- To complement and reinforce the architectural and landscape character of all public and private spaces.
- To contribute to the ease of wayfinding through the development.
- To meet all applicable public and environmental standards including energy conservation.
- To provide a consistent quality of lighting throughout The Village development.
- To avoid adverse impacts such as excessive glare and light spill.
- To reinforce the identity of each component of The Village, including private and public space improvements.

7.7.2 Application

All exterior lighting shall be in conformance with an Exterior Lighting Plan to be prepared by the Developer. All exterior lighting placed within or visible from private and public open spaces will conform to this Lighting Plan. Until this plan is prepared, the following regulations will govern exterior lighting in The Village. The final Exterior Lighting Plan will supersede the requirements of this section.

The Developer(s) must prepare and submit an Exterior Lighting Plan as part of the submission package for review and approval by the City of Oxnard prior to issuance of building permits. This plan shall be based on the regulations of this Section and subsequent documents.

7.7.3 Exterior Lighting Standards

7.7.3.1 *Lighting Design Quality*

- Architectural lighting should articulate and animate the particular building design, as well as provide the required functional lighting for safety and clarity of pedestrian movement.
- Exterior lighting shall illuminate the entrances of buildings and parking areas to promote convenient, safe and easy identification.
- Exterior lighting within private development should compliment

CHAPTER 7.0

Design and Architectural Standards and Guidelines

and not conflict with public lighting.

- Exterior lighting shall compliment and be consistent with the site and building design. The lighting design shall become an integral part of the overall site and architectural concept. Fixtures, light standards and all exposed accessories should be harmonious with the building design and the visual environment. Obtrusive lighting elements which detract from the appearance of their setting shall be avoided.



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- Where fixtures, shades or other elements are exposed, they should contribute to the overall design of the building.
- Exterior building illumination shall be focused at the pedestrian level—storefronts, building entries and comparable locations.
- Special accent lighting may illuminate unique architectural details where appropriate, but such lighting effects which illuminate entire buildings or large portions of buildings will not be permitted where such buildings are prominently visible from adjacent public streets.
- Landscape lighting, including tree uplights, small bare-bulb incandescent lights, “twinkle” lights and other similar lighting effects may supplement the indirect nature of the rest of the Specific Plan Area’s night lighting.

7.7.3.2 *Lighting Spillover*

- Excepting bollard lighting less than 42 inches high, all lighting potentially visible from public areas shall be indirect or shall incorporate a fixture using a baffle or shield. This requirement applies to all lighting, whether originating from public or private portions of the Specific Plan Area.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- Service area lighting shall be contained within the service yard boundaries and enclosure walls. Minimal light spillover should occur outside the services area. The light source shall be screened from the street.
- Building illumination and architectural lighting shall be indirect, with no light source visible. Indirect wall lighting for “wall washing” overhead down lights or interior illumination which spill outside is encouraged.

7.7.3.3 *Lighting Type*

- All night lighting shall have a warm, incandescent appearance; “cool” light sources, such as mercury vapor and low or high-pressure sodium lights, shall not be visible from major streets and preferably should not be used. Illuminated areas shall be localized as much as possible. Light fixtures that broadcast light over large areas, or which are a source of glare, are not permitted.
- Minimize the use of fluorescent, mercury vapor, and high and low-pressure sodium lamp sources in areas where it might be visually prominent.
- Multi-colored lights may only be used sparingly.

7.7.3.4 *Lighting at Parking Lots and Fields*

- Parking areas, access drives and internal vehicular circulation areas in commercial areas shall have pole-mounted lighting fixtures. These lighting fixtures shall be a zero cut-off type, mounted 20 feet atop a square or round metal pole. Each fixture and pole shall have a durable white finish. The light source shall be high-pressure sodium. The parking lot illumination level shall achieve a uniformity ratio of 6 to 1 (average to minimum) with a maintained average of 1 footcandle.

7.7.3.5 *Pedestrian Area Lighting*

- Pedestrian area lighting serves outdoor pedestrian uses such as courtyards and entry ways. Pedestrian area lighting should achieve a uniformity ration of 4 to 1 average to minimum, with an average illumination note exceeding 1.0 foot-candles.
- Private developments shall include exterior lighting of pedestrian walkways. For pedestrian walk lighting, point-to-point lighting is acceptable. The main emphasis in this area should be to clearly identify the pedestrian walkway and direction of travel. Lighting levels should be set relative to the level of security necessary and

CHAPTER 7.0

Design and Architectural Standards and Guidelines

the mounting height of the luminaries. The standards shown below are averages. Higher lighting levels may be necessary to provide for safety, subject to the approval of the City of Oxnard.

- Average conditions: 0.5 foot-candles.
- Security conditions: 9-15 foot tall fixtures: 2.0 foot-candles.
- Security conditions: 15-30 foot tall fixtures: 4.0 foot-candles.

7.7.3.6 Storefront Lighting

- All storefront lighting should be baffled to prevent glare from along the sidewalk or street. Where fixtures, shades, or other elements are exposed, they should contribute to the overall design of the storefront.

7.7.3.7 Lighting in Residential Alleys

- Lighting shall be provided in all residential alleys to contribute to security and safety. Lighting could be provided by a minimum of one wall-mounted fixture per garage, or by small-scale pole-mounted luminaries.

7.7.3.8 Other Standards

- Consider energy conservation in nighttime lighting plans. Plans for the design and operation of

lighting and illumination should be developed consistent with the latest technical and operational energy conservation concepts, such as use of solar energy.

- All exterior lighting shall be maintained on a regular basis to an “as-new” standard to assure that all lighting fixtures, bulbs and elements are in good working order.
- Lighting shall meet all applicable public and environmental standards. Where Specific Plan and City requirements conflict, Specific Plan requirements shall take precedence.
- Outdoor lighting shall comply with Title 24, Part 6, of the California Code of Regulations: California’s Energy Efficiency Standards for Residential and Non-Residential Buildings.
- Unless approved as a specific exemption to this guideline, all outdoor lighting shall be flat lens, full cut off fixtures with the light source fully shielded with the following exceptions:
 - Luminaires with a maximum output of 260 lumens per fixture, regardless of the number of bulbs (equal to one 20-watt incandescent light), may be left unshielded provided that the fixture has an opaque top to keep light from shining directly up.

CHAPTER 7.0

Design and Architectural Standards and Guidelines

- Luminaires that have a maximum output of 1,000 lumens per fixture, regardless of the number of bulbs (equal to one 60-watt incandescent light) may be partially shielded provided the bulb is not visible, and the fixture has an opaque top to keep light from shining directly up.

- Lighting within the physical limits of the area required to be lighted shall not exceed seven foot-candles, nor be less than one foot-candle at any point. A light source shall not shine upon, or illuminate directly any surface other than the area required to be lighted. No lighting shall be of a type or in a location that constitutes a hazard to vehicular traffic, either on private property or on abutting streets. The height of light standards shall not exceed 26 feet. To prevent damage from automobiles, standards shall be mounted on reinforced concrete pedestals or otherwise protected.



The Village

SPECIFIC PLAN



Chapter 8

Administration and
Implementation

CHAPTER 8.0

Administration and Implementation

8.1 INTRODUCTION AND APPLICATION

This Chapter describes the roles, responsibilities and procedures required for the City of Oxnard, the Developer, and other stakeholders to implement The Village Specific Plan in accordance with the Plan’s vision, standards and guidelines.

From a legal perspective, this Chapter also fulfills the requirements of Government Code 65451 (California Planning Law). This code requires that specific plans shall include a program for implementation, including regulations, conditions, programs and additional measures as necessary to carry out the plan.

8.1.2 Application

The Village Specific Plan applies to all lands within the Specific Plan Area boundaries shown in Figure 1.3¹. All development within the Specific Plan boundaries must be consistent with the Specific Plan and the City of Oxnard’s General Plan, and must be approved and granted a permit by the City of Oxnard before development. Within the Specific Plan Area, the Regulations contained within this Specific Plan shall govern development. All standards not specifically addressed by this Specific Plan shall be governed by applicable sections of the City of Oxnard Zoning

Ordinance and Municipal Code. The building, landscaping, and other regulatory illustrations shown in this Specific Plan are intended to be generally in conformance with all applicable Specific Plan regulations and shall serve as a guide to Developer(s) and City Staff when preparing and reviewing more formal development plan submittals.

8.2 REGULATORY OBJECTIVES

This Specific Plan incorporates the following set of regulatory objectives intended to facilitate appropriate development:

1. To assure that all development within The Village is consistent with the planning and design intent, guidelines and standards of this Specific Plan, as well as with other applicable City of Oxnard development and performance standards.
2. To specify land uses and development guidelines within the Specific Plan which are consistent with the City of Oxnard General Plan.
3. To establish, as part of the City of Oxnard General Plan, The Village project area as a “Specific Plan” land use designation, to which more detailed land use definitions and development standards apply to only those areas within The Village Specific Plan.

¹ Note that the Specific Plan boundaries include the proposed 101 Freeway landscaping areas along the property’s northern frontage. These areas are currently owned by the California Department of Transportation.

CHAPTER 8.0

Administration and Implementation

4. To provide a range of development standards and guidelines that are mandatory, recommended, or illustrative in nature.
5. To provide an efficient project review and approval process of formal development plans consistent with The Village Specific Plan.

8.3 ADOPTION, ADMINISTRATION AND RELATED PUBLIC ACTIONS

8.3.1 Specific Plan Adoption

As part of the review and approval process of this Specific Plan, the 2020 General Plan was amended to reflect the revised land uses and land use locations proposed within the Specific Plan. Upon the adoption of the Specific Plan, the Specific Plan was deemed consistent with the provisions of the General Plan. Adoption of the Specific Plan makes the land uses and development standards of the Specific Plan regulatory in nature and equal to, but separate from, the existing regulations contained within the Oxnard Municipal Code and the Oxnard Zoning Ordinance. These actions have created consistency between the General Plan and Zoning Ordinance designations and the Specific Plan.

8.3.2 Additional Approvals

In conjunction with approval of this Specific Plan, several other related public actions are necessary to implement the Specific Plan, including the following:

1. Approval of a General Plan Amendment consisting of changes to the 2020 General Plan Land Use Map designations for the Specific Plan Area and changes to the text of the Land Use Element of the General Plan and the General Plan Land Use Map.
2. Approval of a Zone Change consisting of changes to the existing Zoning designations shown on the City's Zoning Map to those proposed as part of this Specific Plan.
3. Approval of a Master Tentative Tract Map for the Specific Plan area.
4. Approval of a Development Agreement between the Developer and the City of Oxnard.
5. Approval of an Owner Participation Agreement for the owners of The Village Specific Plan Area which is located within the City's HERO Re-development Area.
6. Certification of a Final Environmental Impact Report (FEIR) for the Specific Plan Area.

8.3.3 Specific Plan Administration

The Village Specific Plan shall be administered and enforced by the City of Oxnard Planning Department.

The land use regulations contained within the Specific Plan are designed to be implemented in conjunction with the City of Oxnard Zoning Ordinance. Where the Specific Plan specifies standards or regulations for particular uses, it shall be the regulatory document. Where applicable development standards and regulations are not specified in the Specific Plan, the provisions of the City of Oxnard Zoning Ordinance shall be used to regulate development. Where there is a conflict between the Specific Plan and the Zoning Ordinance standards or regulations, the Specific Plan standards or regulations shall apply.

8.4 IMPLEMENTATION RESPONSIBILITIES

8.4.1 Basic Responsibilities of the Developer

The Specific Plan requires that one Master Developer or a combination of Master Developer and additional Builders/Developers assume the primary responsibility for:

1. Implementing project development according to, and in conformance with, the design intent of the Specific Plan; and initiating and participating in a Design Review process until project completion.

The Master Developer may consist of:

- a) A single entity or combination of entities responsible for residential and commercial development as well as the

open space/public realm components, and infrastructure components.

8.4.2 Project Review and Approval

The City of Oxnard is responsible for administering the project review and approval Processes described in this Chapter. The Master Developer is responsible for initiating and participating in review by the City's Development Advisory Committee (DAC), which conducts a detailed review of each project pursuant to all applicable conditions of approval and provisions of the Specific Plan. DAC evaluation and approval will result in a recommendation to the City Staff confirming the conformance of each individual development proposal with the standards and guidelines of the Specific Plan, the applicable conditions of project development adopted by the City of Oxnard, and the applicable provisions of the City of Oxnard Zoning Ordinance and Municipal Code.

8.4.3 Construction and Maintenance

8.4.3.1 Construction

The Master Developer or a combination of Master Developer and additional Builders/Developers is responsible for construction of the following items:

1. The backbone street system and any related signalization, as shown in this Specific Plan (Figures 2.1, 2.2, 2.3, and 5.2) and the Tentative Tract Map. The term "backbone" refers to

Administration and Implementation

streets and their associated utilities and landscaping in the public or private street right-of-way (excluding alleys) which shall be constructed by the Developer(s) through one or more phases;

2. Landscaping and hardscape as part of the pedestrian and bicycle circulation network (Figures 2.4 and 2.5), lighting and directional signage associated with the backbone street system (Sections 7.6 and 7.7), and the landscaping proposed along the California Department of Transportation Highway 101 right-of-way (Figure 5.13), Union Pacific Railroad frontage (Figure 5.15), Oxnard Boulevard frontage (Figure 5.16 and 5.17), and Ventura Road frontage (Figure 5.18 and 5.29);
3. All wet and dry utilities (including gas, electric, water, sewer, and communication lines) within the public or private street right-of-way (excluding alleys);
4. Drainage and storm water management improvements;
5. Off-street and on-street parking facilities;
6. Parks and open spaces identified in the Specific Plan; and
7. Residential, mixed use, commercial, and transit oriented buildings as identified within the Specific Plan.

8.4.3.2 *Maintenance*

It is anticipated that a master Home Owner's Association (HOA) will be created, along with individual HOA's for specific neighborhoods.

The Master Developer or a combination of Master Developer and additional Builders/Developers has maintenance responsibilities for the following:

1. Streets, lighting, signage, landscaping, parks, and hardscape within the private areas of the Specific Plan shall be maintained in conformance with City of Oxnard standards;
2. Streets, lighting, signage, landscaping, parks, and hardscape within the public areas of the Specific Plan, as identified on Figure 5.1, shall be maintained by the City of Oxnard in conformance with City of Oxnard standards and via a Community Facilities District (CFD); and
3. Drainage and storm water treatment facilities shall be maintained by the Master Developer or a combination of Master Developer and additional Builders/Developers until such time as ownership of the facilities is transferred to the appropriate public entity such as the City of Oxnard or a new entity formed to own and maintain the storm water infrastructure.

8.5 DEVELOPMENT PHASING AND SCHEDULING

8.5.1 Project-Wide Development Phasing

Build-out of The Village Specific Plan is anticipated within five to seven years, based upon a current assessment of future market conditions. **However, the phasing and build-out timelines presented herein are preliminary and subject to change based upon future market conditions.** Construction will include: (1) phased demolition of all existing buildings and utilities, and site grading, (2) construction of roads, wet and dry utilities, drainage improvements, parks and other open space areas, surface and garage parking areas, and (3) construction of residential, commercial, and transit uses.

Completion of construction is anticipated in four development phases, described below and shown in Figure 8.1. The schedule for the start and completion of each phase is dependent on the time required for Oxnard staff review and approval, construction timeframes, and the time required to achieve appropriate market absorption. The details of the phasing plan may change as specific development phases are designed and submitted to the City for final review and approval.

Phase I: includes the demolition and site grading of all areas east of the mobile home park, except the existing bowling alley, the ice skating rink and the northern portions of existing Buckaroo Avenue (See Figure 6.1 Demolition Map). Concurrent with site grading, the Master

Developer or a combination of Master Developer and additional Builders/Developers will construct the basic backbone infrastructure to connect Phase 1 to the existing public infrastructure. The utilities and infrastructure serving the remaining uses (including the mobile home park, bowling alley, and ice skating rink) will be kept in place to allow continued operation during Phase I construction.

Phase I construction will include the development of the essential public and private streets necessary to provide vehicle and pedestrian connections to Oxnard Boulevard and secondary access to Ventura Road. Appropriate interim secondary vehicle access to Ventura Road (as determined by the City of Oxnard Fire Department) will be maintained to all existing uses throughout Phase I construction and until final completion of the proposed public access. Finally, Phase I construction will include up to 600 multi-family residential units, and up to 50,400 square feet of commercial retail space located in Planning Areas 17, 18, 20, and 21 through December 31, 2011. The final permitted number of residential units and commercial square footage permitted under Phase I shall be determined by the sewer and water capacity study. Please refer to Figure 8.1 for additional information.

Phase Ia: includes formal closure of ice skating rink, and bowling alley and demolition of the Wagon Wheel shopping center. Phase 1a also includes construction of infrastructure necessary to support 119 affordable housing units for

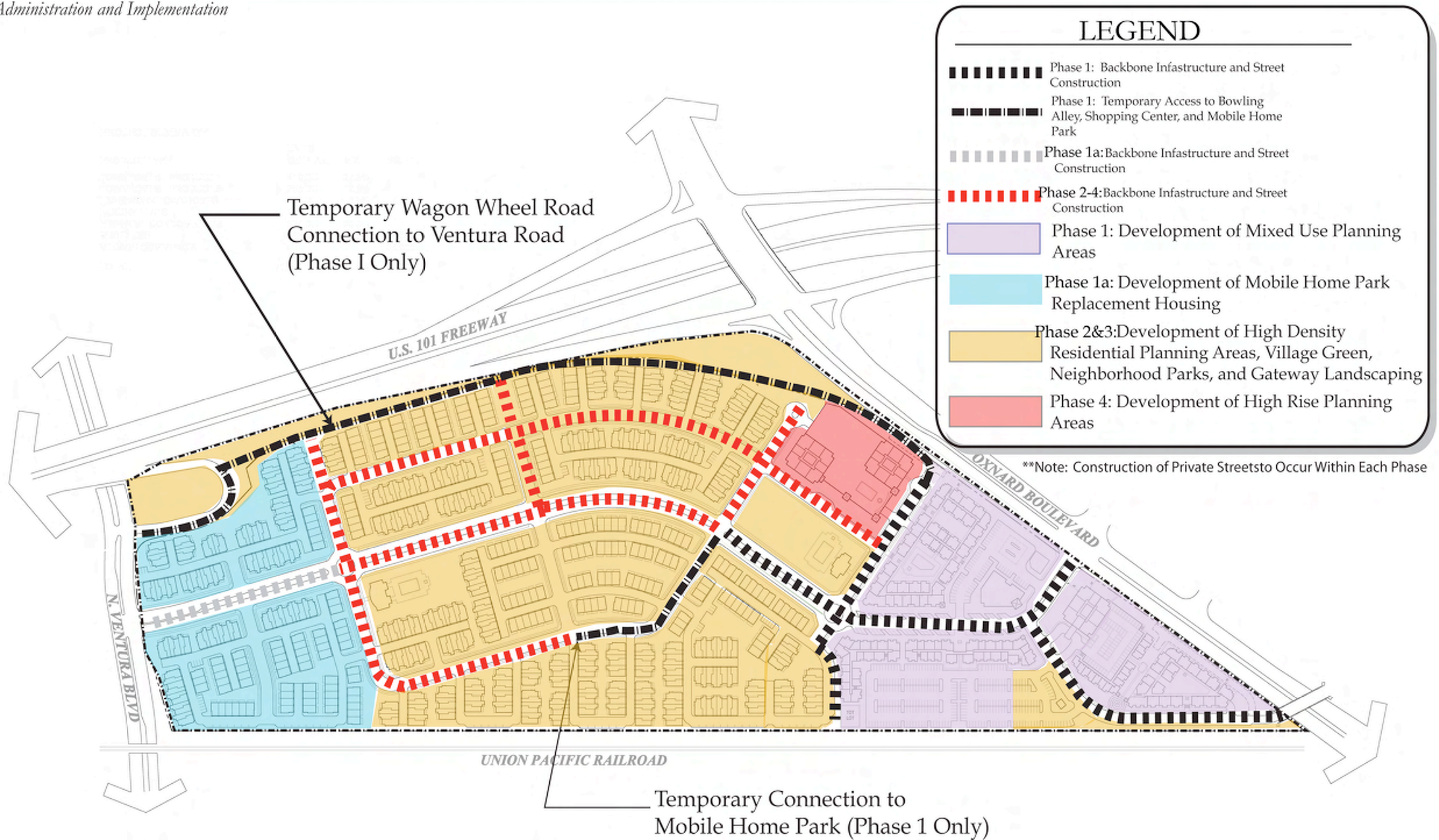
CHAPTER 8.0

Administration and Implementation

mobile home park replacement housing, including but not limited to, wastewater pipelines, recycled water pipelines, water pipelines, drainage improvements, and utilities. Upon completion of demolition and construction of supporting infrastructure, 119 affordable housing dwellings will be constructed for the existing mobile home park residents. Occupancy of these dwellings is likely to occur between January 2012 and December 2013.

Phases II & III: includes formal closure of the mobile home park and demolition of any remaining structures. All remaining portions of the site will be graded and all remaining backbone infrastructure to support site development will be constructed between the boundary of Phase I and Phase Ia, including, but not limited to, public and private streets, wastewater pipelines, recycled water pipelines, water pipelines, drainage improvements, and utilities. Phases II and III include the construction of the remaining 574 dwellings, parks, and open spaces within High Density Residential Planning Areas 4-15. Occupancy is anticipated between from January 1, 2012 through December 31, 2013. Phase III also includes the construction of freeway edge landscaping along the US 101 Freeway and the Transit Center.

Phase IV: will include construction of 207 dwellings within the high-rise towers within the High-Rise Planning Area 16 and their supporting recreational areas with completion and occupancy after January 1, 2014.



8.5.2 Land Use Flexibility

Each Planning Area within the Specific Plan specifies a maximum number of allowable units and a density range. Figure 2.1 and Table 2.2 and 2.3 in Chapter 2 list each Planning Area's density range, target unit count, and target commercial square footage.

Within each Planning Area, a Developer may choose to construct dwellings within the range of permitted densities established in Table 2.2 or 2.3 to the extent that the total unit count does not exceed 1,500 and the target unit count for each dwelling type is not exceeded. This Specific Plan permits the transfer of dwelling units up to the maximum allowed density in all Planning Areas as specified on Figure 2.1- Land Use Map. The addition or subtraction of residential units within a Planning Area or group of Planning Areas as part of a dwelling unit transfer cannot exceed: (1) the total maximum number of units permitted (i.e. 1,500) within the Specific Plan, (2) the target number of units for each dwelling type, or (3) the maximum density permitted within each Planning Area, as established in Table 2.2 or 2.3.

Any land use modifications within a particular Planning Area in accordance with the above described dwelling unit transfer program would be subject to review and approval by the Planning Manager and Development Advisory Committee (DAC).

8.6 FINANCING AND FUNDING

8.6.1 Financing and Funding Principles

The policies described in this section shall govern the funding of facilities and public services for The Village Specific Plan. The principle underlying these policies is that The Village should not pose an unreasonable financial burden or obligation on the City of Oxnard or other jurisdictions beyond that which may be necessary to meet State and/or City requirements related to maintaining public safety and providing such services as school, fire and police.

Accordingly, the Master Developer or a combination of Master Developer and any additional Builder/ Developers, will pay the full cost to construct the infrastructure needed to serve The Village Specific Plan, to the extent feasible. The Master Developer will fund the costs of mitigating adverse impacts on the City's existing utility and transportation infrastructure, and if necessary, provide for a fair allocation of such costs among the various proposed land uses. The City of Oxnard shall provide pro-rata reimbursements for any improvements determined to be regional in nature as stipulated in a Development Agreement and/or Owner Participation Agreement between the City of Oxnard and the Developer.

8.6.2 Financing and Funding Policies

The following policies govern the financing of infrastructure and public services for The Village Specific Plan in accordance with the above principles:

On-Site Public Infrastructure: The Master Developer or a combination of Master Developer and additional Builders/Developers will fund the full cost of constructing public infrastructure and public services within the Specific Plan Area.

Off-Site Public Infrastructure: The Master Developer or a combination of Master Developer and additional Builders/Developers will fund the “fair share” costs of public infrastructure outside of the Specific Plan Area required to support development within The Village. Construction of the various off-site circulation improvements can be funded by a combination of sources. These can include City, County, and State resources, as well as developer financing. A CFD can be utilized in place of, or in combination with, a set fee or improvement cost.

Any improvements to the major roadways fronting the Specific Plan Area will be paid for by a combination of developer financing, and any transportation impact fees that cover the entire Specific Plan Area. A CFD may be considered in place of, or in combination with, the fees or improvement cost. Landscape maintenance within the public rights-of-way may be funded by a Community Facilities District (CFD).

Landscape and Lighting: A CFD may be formed to address the maintenance of the variety of proposed landscaped areas within The Village Specific Plan. Upon formation, the CFD will be administered by the City of Oxnard. The City of Oxnard will be responsible for overseeing the maintenance and repair of public common areas and related landscaped areas within the Specific Plan Area.

Schools: The Master Developer or a combination of Master Developer and additional Builders/Developers shall pay all applicable school fees, as permitted under SB 50, for construction of schools in the City of Oxnard, or as specified in a School Facilities Mitigation Agreement between the School District(s) and the Developer. A CFD may be formed to provide a source of mitigation funding to the School District(s).

Private Streets, Internal Walkways, Water Features, Parks and Open Space: The Village Specific Plan’s financing plan will develop a system to fairly distribute the cost of constructing and maintaining the various private streets, walkways, trails, water features, parks, and open space elements within the Specific Plan Area. Construction of these elements will be completed by the Developer, and may be maintained using funds generated by a CFD. Private landscape area maintenance will be managed by property owner’s association(s).

Maintenance of public landscaped areas will be provided by the City of Oxnard or the Developer via a CFD. For planning purposes, Figure 5.1 illustrates the parties

CHAPTER 8.0

Administration and Implementation

responsible for maintaining public and private landscaped areas.

8.7 PROJECT AND SUBDIVISION MAP APPROVAL PROCESS

8.7.1 Approach

A “project” is defined in this Section as a development of new commercial and/or residential buildings and their related facilities within a portion of the Specific Plan Area by the Master Developer and/or additional Builder(s)/Developer(s). Any project within The Village Specific Plan Area must be reviewed and approved by the City of Oxnard Planning Manager, or other appropriate decision making body (such as DAC) as determined by the Planning Manager, prior to issuance of building permits. These approvals will assure that each project will be consistent with the adopted provisions of this Specific Plan, the 2020 General Plan, the City’s Zoning Ordinance, the City’s Municipal Code, and other instruments of public policy.

A Tentative Subdivision Map for the Specific Plan Area as a whole, and each subsequent project or phase, must be reviewed and approved by the City of Oxnard. The review and approval process for a subdivision map submitted as part of The Village Specific Plan shall be established in accordance with the City of Oxnard procedures, as outlined under Chapter 15 of the Oxnard Municipal Code.

8.7.2 Plan Approval Requirements

In terms of City project review and approval, projects fall into one of two categories:

1. Project applications requiring only a Development Design Review (DDR) permit subject to review and approval by the Planning Manager and/or DAC;
2. Projects requiring review by the Oxnard Planning Commission and/or City Council as determined by the Planning Manager. Note that a list of Permitted Uses for each Planning Area is included in Chapter 2, Table 2.4 of the Specific Plan.

8.7.2.1 Requirements for Project Review

1. New Development on Vacant Parcel(s): New development on vacant parcel(s) may be approved by the Planning Manager and/or DAC through a Development Design Review Permit. This type of project would be approved provided that it contains land uses identified within Chapter 2, Table 2.4 and is consistent with all other applicable provisions of the Specific Plan.
2. Use Changes on Developed Parcel(s): Projects which involve uses other than those uses listed in Chapter 2, Table 2.4 on developed parcel(s) may be approved by the Planning Manager and/or DAC through a zone clearance.

CHAPTER 8.0

Administration and Implementation

Minor Deviation: Minor Deviations may be approved by the Planning Manager and shall require a minor modification permit. The following modifications fall within the parameters of a minor deviation:

- Deviations within 15% of numerical development standards for setbacks, building height, building additions, lot coverage, lot area, lot width, lot depth, and parking requirements; and
- Specific modifications of a similar nature to those listed within minor modifications, which are deemed by the Planning Manager and/or DAC to be in keeping with the spirit and intent of the Specific Plan, and are in conformance with the 2020 General Plan.

4. Major Deviations: Projects that do not qualify under the minor deviation procedure shall be considered major modifications. Major modifications require the submittal of a major modification permit application and would be considered by the Planning Commission. The following would fall under the parameters of a major deviation:

- Deviations exceeding the 15% numerical standards for setbacks, building height, building additions, lot coverage, lot area, lot width and lot depth, and parking requirements.

5. Specific Plan Amendment: An amendment to the Specific Plan would be required when text and / or map changes or additions are proposed to the Specific Plan document. Examples of this include, but are not limited to, the reconfiguration of streets as illustrated on Figure 2.1, 2.2, or 5.2, or a request to increase the total number of dwelling units above 1,500. A Specific Plan amendment requires final approval by the City Council.

8.7.3 Plan Approval Process

The following review process is intended as a guide for all parties involved in reviewing and approving specific development proposals submitted as part of this Specific Plan. Although this guide could be applied to all properties, actions, and development applications within the Specific Plan Area, it is not meant to supersede all applicable review and approval procedures adopted by the City of Oxnard as part of the Zoning Ordinance and/or Municipal Code. They shall remain in full force and effect.

Step 1: Pre-Submittal Meeting

Project applicants should meet with City Planning Staff to verify how the Specific Plan applies to a particular project area and/or parcel and to clarify and/or answer questions of the potential applicant. Applicants are encouraged to consult with City Staff regarding proposed designs (site and buildings) prior to submittal of a formal development application and corresponding fee deposits.

Step 2: Formal Development Plan Submittal

The following documents shall be submitted with the City's development application and the corresponding fee deposits:

Tentative Tract Map identifying all blocks, thoroughfare types, lots for development or parks, the plotting of all building types for the various types of lots, and all required engineering information per the City of Oxnard Public Works Department.

Proposed submittals where building construction constitutes an integral part at the time of submittal shall have architectural plans identifying the following:

- a) Proposed building types consistent with those represented in Chapters 3 and 4 in the Specific Plan. If building types are proposed that are not identified in the Specific Plan, such designs will be reviewed for compatibility by the Planning Manager.
- b) Along with the selected building types, preliminary architectural floor plans, exterior elevations, colors and materials, infrastructure plans, and landscaping plans shall be submitted to the City for administrative review for consistency with the applicable provisions of this Specific Plan. Once the applicable development permit has been granted, the plans may be submitted for review and processing by the Building and Safety Department.

CHAPTER 8.0

Administration and Implementation

Step 3: Development Design Review Permit

The DAC shall review all applications and provide comments for consideration by the project applicant and verify condition compliance.

In the event that the application is not fully consistent with the applicable provisions of this Specific Plan or the conditions of approval, the applicant shall be informed of this in writing as to the specific reasons and be given the opportunity to address said item(s). Upon re-submittal, the committee shall review only those items deemed inconsistent with the Specific Plan and/ or conditions of approval, unless the revisions made to the application resulted in a revision to any other portion of the application, in which case that portion may also be reviewed as part of the resubmitted application.

Step 4: Planning Commission and/or City Council Public Hearings

In the event that a project requires approval of a Special Use Permit (SUP) as defined in the Oxnard Zoning Ordinance, or a project requires modification, the project applicant shall submit a development application and all required supporting documentation to the Planning Department for review and consideration by the appropriate decision making body, as required by the City's Municipal Code.

In the event that project issues cannot be resolved during Development Design Review committee meetings, or if components of a particular development application require discretionary review and approval pursuant to the City of Oxnard Municipal Code, the City Staff shall prepare a report summarizing the proposed development and provide a recommendation for consideration by the Planning Commission. Upon consideration, the Commission may approve the request, continue the matter for more information, or deny the application due to reasons stated at the hearing.

In the event that a project requires a modification to the Development Agreement and/or Owner Participation Agreement, the project applicant shall submit a development application and all required supporting documentation to the Planning Department for review and consideration by the appropriate decision making body, as required by the City's Municipal Code.

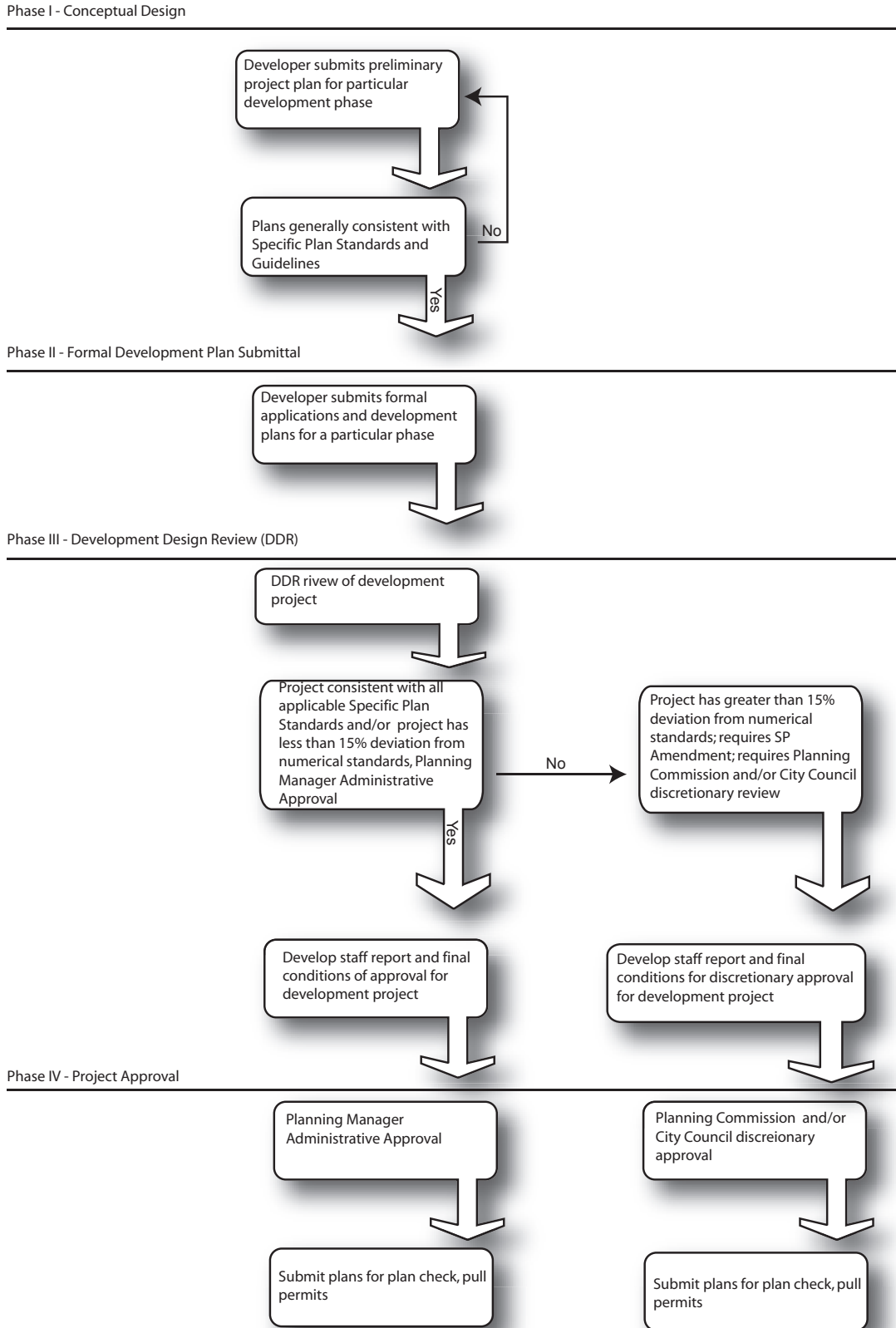
8.7.3 Appeals

The Developer shall have the right to appeal any decision rendered by the DAC and/or the Planning Manager as part of Development Design Review to the Planning Commission.

The Developer shall subsequently have the right to appeal any decision rendered by the Planning Commission to the City Council.

Figure 8.2 illustrates the expected review process governing future development of the Specific Plan.

Administration and Implementation



Review Process Flow Chart

8.8 OTHER ISSUES

8.8.1 Effects of a General Plan Amendment on the Specific Plan

Adoption of amendments to the 2020 General Plan or a General Plan Update by the City will not necessarily require amendment of the Specific Plan. However, any subsequent discretionary approval or amendment to the Specific Plan must be consistent with the General Plan, as amended and/or updated, except to the extent that such change in the General Plan deals with matters in which the Developer shall possess vested rights under the terms of a Development Agreement and/or Owner Participation Agreement entered into between the Developer and the City.

8.8.2 Environmental Issues

In conjunction with approval of The Village Specific Plan, The City of Oxnard certified the Project Environmental Impact Report pursuant to the provision of the California Environmental Quality Act.

The Mitigation Monitoring and Reporting Program (MMRP) is included as part of the Certified Final EIR prepared for this Specific Plan. The MMRP and all of its requirements are thus incorporated by reference into this Specific Plan and must be implemented as part of project development.



The Village

SPECIFIC PLAN



Chapter 9

General Plan Consistency
Analysis

CHAPTER 9.0

General Plan Consistency Analysis

9.1 INTRODUCTION

California Government Code (Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. As a result, specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan.

The City of Oxnard General Plan contains the following elements: Growth Management, Land Use, Circulation, Public Facilities, Open Space/Conservation, Safety, Noise, Economic Development, Community Design, Parks and Recreation, and Housing.

The Village Specific Plan is consistent with and implements the Oxnard 2020 General Plan. The Village Specific Plan has been prepared in conformance with the relevant goals and objectives of the General Plan. A discussion of how The Village Specific Plan relates to, and is in conformance with, the applicable goals and objectives identified in the City's General Plan is provided below.

9.2 GROWTH MANAGEMENT ELEMENT

GOAL: Sensible urban growth based on the ability to provide the necessary governmental services and municipal utilities.

GOAL: Maintain the quality of life desired by the residents of Oxnard.

GOAL: Orderly growth and development that is consistent over the life of the 2020 General Plan, fostered by the CURB.

Objective: Insure that public services and facilities are in place at the time of need or prior to the time new development occurs.

Objective: Insure that new development avoids or fully mitigates impacts on air quality, traffic congestion, noise, and resource protection.

Objective: Monitor the pace of growth and development throughout the City to assure achievement of the goals and policies of this 2020 General Plan.

Objective: Maintain the City's fiscal integrity and insure that revenue generated by the new development is sufficient to offset City costs.

Objective: Create an appropriate balance between urban development and preservation of agricultural uses with the Planning area. Development exclusively within the CURB while leaving the balance in Resource Protection, Open Space or Agricultural designations is presumptively an appropriate balance.

Objective: Insure that areas annexed to the City share equitably in the costs of all necessary municipal improvements.

Project Consistency: The Village Specific Plan contemplates the re-use of a previously developed site within the City of Oxnard and the City's HERO Redevelopment District. The plan utilizes smart growth design principles to create a new mixed-use pedestrian friendly, transit oriented community in keeping with the surrounding land uses. High quality architecture and landscape designs for all components of the project are proposed to insure that a high quality environment for all visitors and residents.

Previous development of the subject site and the surrounding areas has established the majority of the regional infrastructure to support the proposed development. Future project-related connections will be established without exceeding the capacity of municipal services, or if necessary, regional improvements will be constructed as part of Specific Plan build-out to significantly reduce municipal service impacts.

The Village Specific Plan is located entirely within the CURB boundary and therefore, no impacts to agricultural uses will occur. Development will be phased according to the phasing plan provided in the Specific Plan to ensure that adequate infrastructure and municipal services are in place prior to issuance of a building or occupancy permit. A comprehensive infrastructure plan was completed by the City of Oxnard as part of The Village Specific Plan and concluded that with completion of the recommended

improvements, no significant impacts to public services would result.

The Specific Plan proposes residential, commercial, and transit related uses and will contribute its statutory share towards the improvement of school facilities, and the police and fire services required to serve the proposed uses.

The Specific Plan provides substantial financial benefit to the City in the form of tax-increment revenue and fee payments generated by the conversion of the subject site from under-utilized commercial/industrial uses to a thriving mixed-use community.

9.3 LAND USE ELEMENT

GOAL: A balanced community meeting housing, commercial and employment needs consistent with the holding capacity of the City.

GOAL: Preservation of scenic views, natural topography, natural amenities, and air quality.

GOAL: A balance between jobs and housing within a reasonable commuting distance from each other.

Objective: Limit the urbanized area of the City and facilitate a permanent greenbelt between Oxnard and neighboring cities.

Objective: Provide a variety of housing types throughout the City.

CHAPTER 9.0

General Plan Consistency Analysis

Objective: Preserve permanent agricultural land within the Oxnard Planning Area.

Objective: Provide for adequate space for schools, libraries, park and recreation areas, and the expansion needs of public facilities to enhance the quality of life for all citizens.

Objective: Facilitate the availability of an ample range of affordable child and senior care services.

Objective: Ensure that all new development will be consistent with the Ventura County Air Quality Management Plan and other regional plans.

Objective: Promote improved coastal access and recreation opportunities.

Objective: Support the development of land uses that enhance the City's efforts as a recreational travel and conference destination.

Objective: Create new job opportunities tailored to the skills of the City's labor force, particularly unemployed residents.

Objective: Encourage the development of mixed uses in appropriate areas to reduce commuting.

Project Consistency: The Village Specific Plan proposes a range of higher density housing types (including affordable housing for families and senior citizens) recreational opportunities, a sub-transportation center, and neighborhood serving commercial-retail configured in a way that encourages pedestrian activity over automobile usage. Locating residential dwellings adjacent to essential commercial services, job opportunities, recreation, and alternative modes of transportation project helps to preserve the remaining undeveloped land (and its environmental resources) within the City of Oxnard, improve air quality by reducing both local and regional automotive trips to and from the proposed development and nearby developments, and improve the jobs/housing balance by creating opportunities for residents to live, work, and play within their community.

The Village Specific Plan is located near the northwestern edge of the City of Oxnard and along Ventura Freeway and Oxnard Boulevard (State Highway 1). To take advantage of its excellent freeway visibility, The Village Specific Plan proposes enhanced landscaping treatments and City signage along its northern, western and eastern frontages that will serve as a beautiful gateway into Oxnard. These landscaped areas will also serve as a buffer between the City of Oxnard, the Santa Clara River and the City of Ventura.

Surrounding land uses consist of commercial and office buildings as part

CHAPTER 9.0

General Plan Consistency Analysis

of the Oxnard Town Center (River Park Specific Plan Area) to the north, the Esplanade Mall and Oxnard Financial Plaza to the east, residential development to the south, and the Santa Clara River to the west. Compatibility with and connections to adjacent uses has been carefully considered as part of The Village Specific Plan. The Specific Plan's mix of uses and architectural character is compatible with adjacent uses. Pedestrian and vehicle connections to the surrounding uses will be improved as part of future development.

9.4 CIRCULATION ELEMENT

GOAL: A transportation system that supports existing, approved and planned land uses throughout the City while maintaining a level of service "C" on all streets and at all intersections.

GOAL: A public transportation system that serves the needs of residents and workers of Oxnard.

Objective: Minimize conflicts between automobiles, bicycles and pedestrians.

Objective: Reduce congestion at major intersections within the City of Oxnard.

Objective: Minimize vehicle miles traveled.

Objective: Achieve a level of service "C" on all City roads where feasible, subject to necessary environmental review.

Objective: Reduce dependency on automobile use for travel needs and increase the use of alternative forms of transportation as a means of reducing energy consumption and vehicle emissions.

Objective: Increase transit rider ship through improved local transit service.

Objective: Develop street designs, install signals and signs, and remove unnecessary all-way stops to minimize interruption in traffic flow.

Objective: Provide a City-wide system of safe, efficient and attractive bicycle routes for commuter, school and recreational use.

Objective: Increase public transportation service to areas of high utilization, such as military bases, commercial centers, business and industrial parks, and other work areas.

Project Consistency: The Village Specific Plan was carefully designed to encourage pedestrian activity over automobile usage by including a variety of commercial uses and amenities within $\frac{1}{4}$ to $\frac{1}{2}$ mile of all planned residential dwellings. The mixing of land uses combined with an

CHAPTER 9.0

General Plan Consistency Analysis

interconnected, pedestrian friendly street network, and convenient access to mass transit via the proposed sub-transportation center will provide safe and efficient vehicle/pedestrian circulation while reducing the need to travel by car to reach essential goods, services, employment centers, and recreational amenities.

To promote the use of mass-transit, The Village Specific Plan includes the development of a Sub-Transportation Center. This facility will provide a bus-rapid transit station, 50 designated parking spaces for a park and ride, and a future metrolink stop. In addition, The Village is currently developing a comprehensive Transportation Demand Management (TDM) program involving residents and businesses within northern Oxnard. The goal of this program is to provide convenient public/private transportation services to local and regional areas of high utilization, such as job centers and commercial centers. Participants in this program are anticipated to include Riverpark, Channel Islands Center, Esplanade, Topa Financial Plaza, Southbank neighborhood, and El-Rio West neighborhood.

The Village Sub-Transportation Center is also planned to include multi-modal transportation solutions designed to provide convenient access to car-sharing, and personal electric vehicles.

Successful implementation of the programs discussed above will significantly reduce potential project

related vehicle emissions and traffic congestion within Oxnard and beyond.

9.5 PUBLIC FACILITIES ELEMENT

GOAL: Public facilities and services adequate to serve existing and future development within the City's Urban Service Area.

Objective: Ensure a water distribution and storage system adequate for existing and future development.

Objective: Ensure adequate sanitary sewer and waste water treatment plant capacity to accommodate existing and future development.

Objective: Reduce solid waste requiring disposal at local landfills and encourage recycling.

Objective: Provide adequately sized storm drain systems to accommodate existing and future needs.

Objectives : Provide adequate police and fire facilities.

Project Consistency: The Specific Plan area is currently developed with commercial and industrial uses. As a result, the full spectrum of wet and dry utilities necessary to serve future development is already in place. As part of any future development plans, the existing on-site utilities will be analyzed to determine the current capacity and will be either abandoned

CHAPTER 9.0

General Plan Consistency Analysis

or removed, and replaced with infrastructure of appropriate size and capacity to serve the proposed uses. Future development of the subject site will be phased to ensure that adequate infrastructure and municipal services are in place prior to issuance of an occupancy permit. Waste disposal facilities within The Village will be provided consistent with City of Oxnard standards, which encourage recycling through the use of household recycling bins.

The subject site is currently almost 100 percent impervious and is located in a highly urbanized setting. On-site runoff currently flows to the southwest via overland flow and empties into an existing culvert before entering the Santa Clara River. No formal storm drain system exists on the site. Development of The Village Specific Plan will establish a formal storm drain system and provide the required stormwater treatment facilities to ensure compliance with the City's drainage requirements and the water quality requirements established by the Clean Water Act. These improvements are estimated to reduce site runoff by at least 25% and improve the quality of stormwater currently discharging into the Santa Clara River.

Future development of The Village will be in conformance with Fire Department access requirements and building separation requirements. All structures will be oriented in a configuration that discourages crime and vandalism, thereby reducing the potential project

related burden on the Oxnard Police Department and Fire Department.

9.6 OPEN SPACE & CONSERVATION ELEMENT

GOAL: Maintenance and enhancement of natural resources and open space.

Objective: Manage water resources to prevent overdraft and loss of water quality.

Objective: Provide adequate open space areas to satisfy the current and future recreation needs of the City.

Objective: Manage urban development to protect open space areas that provide for public health and safety.

Objective: Protect and enhance areas of cultural and historic significance.

CHAPTER 9.0

General Plan Consistency Analysis

Project Consistency: The Village Specific Plan proposes the re-development of a 63-acre site within an urbanized area of Oxnard. As a result, it will not impact existing open space or agricultural areas. A Water Supply Assessment and Infrastructure Impact Report was completed for the proposed plan, and concluded that the City of Oxnard has a sufficient supply of water, and has sufficient infrastructure to meet the needs of future development and the citizens of Oxnard. To further reduce the plan's impact on water resources and other infrastructure components, recycled water will be used to irrigate common landscaping areas and infrastructure improvements will be constructed as required.

The Village Specific Plan includes approximately six acres of community recreation facilities and open space to satisfy the current and future needs of the Village residents. In addition to parks, landscaped corridors, pathways, and pocket parks are also proposed, which will connect the proposed residential uses to the commercial services and recreation areas located in both the Village Specific Plan and the Riverpark development. In addition, The Village developers will work with the City of Oxnard to develop the Santa Clara River Trail along the western portions of the Specific Plan.

9.7 SAFETY ELEMENT

GOAL: Maintenance and enhancement of a safe community.

Objective: Manage urban development to protect areas subject to geological hazards.

Objective: Minimize damage to public and private property from flooding.

Objective: Provide effective and efficient fire protection services.

Objective: Provide effective and efficient police protection services.

Project Consistency: The Village Specific Plan area is not located within close proximity to any known geologic hazards. However, the potential seismic hazards inherent to central and southern California will be addressed by conforming to City and State building regulations and site specific geologic and seismic hazard mitigation measures.

Future development proposed as part of The Village Specific Plan will be in conformance with Fire and Police Department service requirements. Opportunities for on-site surveillance will also be provided.

9.8 NOISE ELEMENT

GOAL: A quiet environment for the residents of Oxnard.

Objective: Provide acceptable noise levels for residential and other noise-sensitive land uses consistent with State guidelines.

CHAPTER 9.0

General Plan Consistency Analysis

Objective: Protect noise sensitive uses from areas with high ambient noise levels.

Objective: Integrate noise considerations into the community planning process to prevent noise/land use conflicts.

Project Consistency: The Village Specific Plan is bordered on two sides by high-level noise producers, the Ventura Freeway and the Union Pacific Railroad. As a result, appropriate setbacks and noise reducing mitigation measures (such as sound walls, berming, building walls, and sound reducing windows) are included as part of the project design in order to reduce and/or avoid significant noise impacts.

Only on-site uses considered compatible with the surrounding residential uses are proposed within The Village Specific Plan.

9.9 ECONOMIC DEVELOPMENT ELEMENT

GOAL: A stable, diversified, and well-balanced economy.

GOAL: Optimum utilization of natural and man-made resources.

GOAL: A variety of economic opportunities throughout the City.

GOAL: A revitalized downtown area of the City.

GOAL: Quality child and senior care services for all in the community.

Objective: Enhance the City's quality of life through better career opportunities, increased diversity of leisure, cultural, and recreational opportunities and upgraded level of public facilities and amenities.

Objective: Enhance Oxnard's image as a progressive city that is responsive to quality growth.

Objective: Identify and attract high quality commercial, retail and industrial businesses to Oxnard that are compatible with the community's business climate and that are not detrimental to the existing local economy.

Objective: Stimulate small businesses.

Objective: Improve transportation for the Oxnard Plain, including air, land and water.

Objective: Seek a complementary rather than competitive role in the economic development of the City's downtown.

Objective: Reduce the unemployment and under employment of Oxnard residents by providing a variety of local jobs.

CHAPTER 9.0

General Plan Consistency Analysis

Objective: Reduce the outward migration of the Oxnard work-force by creating more local jobs.

Objective: Increase the per capita income of City residents.

Objective: Increase Oxnard's share of regional taxable sales to enhance the fiscal base of the City.

Objective: Improve the City's economic base, so the necessity of charging a variety of infrastructure fees for new development can be reduced.

Objective: Improve communication between local businesses and governmental agencies to improve the business climate and to encourage the retention and expansion of local businesses.

Project Consistency: The Village Specific Plan provides a wide variety of land uses intended to stimulate balanced growth of sufficient scale without impacting undeveloped land or the City's downtown business, generate a range of employment opportunities (such as retail and office) adjacent to residential uses, improve access to the City's transportation services by providing an on-site Transportation Center, and generate significant revenue for the City of Oxnard through redevelopment (tax-increment revenue), increases in sales tax revenue, and payment of fees.

The wide range of attractive housing types within the Specific Plan area will facilitate in the attraction and retention of the workforce population within Oxnard.

9.10 COMMUNITY DESIGN ELEMENT

GOAL: A unified and high quality visual image for the City.

GOAL: A thoughtful and sympathetic relationship between the built environment and the natural environment.

Objective: Maintain the unique coastal and agricultural character of Oxnard.

Objective: Preserve the visual identity and character of existing neighborhoods.

Objective: Preserve the City's unique natural features and historic structures.

Objective: Revitalize areas of the City that are currently deteriorated or detract from the visual quality of the City.

Objective: Achieve quality architectural and landscape architectural design that recognizes its surrounding natural environment.

Objective: Upgrade major entryways to the City with landscaping

CHAPTER 9.0

General Plan Consistency Analysis

and/or signage to enhance the City's image and sense of place.

Objective: Enhance the visual identity of the City's activity nodes.

Objective: Preserve important view corridors.

Project Consistency: The Village Specific Plan is located within the Historic Enhancement and Revitalization of Oxnard (HERO) project area. The current condition of the site includes a number of abandoned buildings, under utilized commercial and industrial areas, and a mobile home park. The Village will turn the existing blighted site into a beautiful and vibrant mixed use neighborhood. The Village Specific Plan is designed as a model of "Smart Growth" within a relatively urban setting.

European village and local architectural themes, pedestrian friendly streets, beautiful gateway land-scaping, recreational amenities, and a vibrant mixed use retail core within walking distance of one another will create a landmark project for future residents of The Village and the City of Oxnard.

9.11 PARKS AND RECREATION ELEMENT

GOAL: A variety of quality recreation facilities and resources for Oxnard residents.

Objective: Expand the variety of park types developed by the City.

Objective: Build sufficient Neighborhood Parks, Community Parks and Special Purpose Facilities to meet the needs of the future residents of the City by the year 2020.

Objective: Construct at least two sport and fitness centers with indoor gymnasiums by the year 2020.

Objective: Develop another full-time senior citizen center by the year 2020.

Objective: Determine the ultimate use of the practice field facilities currently used by the L.A. Raiders, which occupy almost 4 acres adjacent to the River Ridge Golf Course/Radisson Hotel.

Objective: Reduce overuse of neighborhood parks where possible.

Objective: Create a physical link for pedestrian and bicycle traffic between facilities.

Objectives : Provide all Oxnard residents with access to natural/scenic areas such as the Santa Clara River green belt, Ormond Beach, and Oxnard Dunes.

Objective: Rehabilitate deteriorated facilities.

Objective: Channel community interests and neighborhood ride into a concern for maintaining

CHAPTER 9.0

General Plan Consistency Analysis

neighborhood facilities in good condition.

Project Consistency: The Village Specific Plan includes a wide variety of passive and active recreational opportunities to meet the needs of future residents of the Village Specific Plan. These include, but are not limited to, a Village Green, with community pool and community center, Neighborhood Park with community pool, pool house and child's play area, and small neighborhood pocket parks and gardens. In addition, the planned pedestrian and bicycle network will provide access to the existing Santa Clara River green belt and other regional recreational facilities located in the Riverpark community. The proposed on-site Class I, and Class II bicycle paths will interconnect with the proposed Class I bicycle paths along Oxnard Boulevard and Ventura Avenue. Convenient pedestrian access to neighboring recreation facilities will be provided via an onsite and off-site pedestrian network.

9.12 HOUSING ELEMENT

GOAL: Meet the existing and projected housing needs of all Oxnard's residents while also maintaining an economically sound and healthy environment.

Objective: Maintain and improve the quality of existing housing and neighborhoods.

Objective: Provide increased opportunities for the construction of quality new housings.

Objective: Expand and protect housing opportunities for lower-income households and special needs group.

Objective: Maintain and improve the quality of existing housing and neighborhoods.

Project Consistency: The Village Specific Plan proposes a diverse high quality and higher density housing stock that will meet the existing and projected housing needs of all income levels of Oxnard residents. These more affordable housing types include townhouses, condominiums, live/work dwellings, and high-rise condominiums. In addition, 15% of the total proposed on-site residential dwellings will be dedicated to very low, lower, low, and moderate income families within the Wagon Wheel Trailer Lodge and the City of Oxnard. The pedestrian oriented nature of the community, the proximity to recreational opportunities, and the location of residential uses adjacent to neighborhood commercial services encourages a healthy lifestyle.